

Phase I and II Architectural History Investigations for the
Willmar Connection and Industrial Access Project,
City of Willmar and Willmar Township,
Kandiyohi County, Minnesota

Submitted to:
Minnesota Department of Transportation
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MANAGEMENT SUMMARY

A public-private partnership including the city of Willmar, Kandiyohi County, the Kandiyohi County and City of Willmar Development Commission, the Minnesota Department of Transportation (MnDOT), and the BNSF Railway Company is proposing a new rail connection between two existing railroad main lines in Willmar, Kandiyohi County, Minnesota, which will require the construction of new crossings at County Road 55, Trunk Highway 12, and Trunk Highway 40. The crossings at the trunk highways will involve vertical and/or horizontal road realignment. This project, the Willmar Connection and Industrial Access Project (Willmar Wye Project), is receiving Department of Transportation funds through the Transportation Investment Generating Economic Recovery (TIGER) competitive grant program, and is therefore subject to review under Section 106 of the National Historic Preservation Act of 1966, as amended. The Federal Railroad Authority (FRA) is the lead federal agency for delivery of physical construction, and the Federal Highway Administration (FHWA) is the lead federal agency for completing the project environmental document, including Section 106 review. FHWA delegates authority for compliance with Section 106 to the MnDOT Cultural Resources Unit (CRU). To assist with their review, the MnDOT CRU contracted with Deco Cultural Services LLC (Deco) to conduct Phase I and II architectural history investigations of the project area of potential effects (APE). The APE was determined by the MnDOT CRU Project Manager, and it comprises approximately 2,712 acres located in Sections 7, 8, 16-20, 29, and 30 of T119N, R35W. The objective of the Phase I investigation, which included literature review and field survey components, was to identify known historic properties and any properties that are potentially eligible for listing in the National Register of Historic Places (National Register) within the APE.

The Phase I architectural history investigation was performed in January to March of 2016, and it identified one property previously determined eligible for listing in the National Register: the St. Paul and Pacific Railroad Mainline: St. Anthony to Breckenridge RR Corridor Historic District (HE-MPC-16387). One property, County Ditch No. 10 (KH-DTC-002), was identified as potentially eligible by the MnDOT CRU Project Manager prior to the Phase I survey. Beyond County Ditch No. 10, a total of 25 properties 45 years in age or older were surveyed, 24 of which were recommended as not eligible for listing in the National Register. The remaining property, the Willmar and Sioux Falls Railway Company main line (XX-RRD-038), was recommended as being potentially eligible for listing in the National Register. Deco therefore conducted Phase II evaluations of County Ditch No. 10 and the Willmar and Sioux Falls Railway Company main line to determine their National Register eligibility. The Phase II evaluations were completed in the spring of 2016. Andrea Pizza served as Principal Investigator for both phases.

Based on the Phase II evaluations, County Ditch No. 10 and the Willmar and Sioux Falls Railway Company main line are recommended as not eligible for listing in the National Register due to a lack of significance; therefore, with the exception of the St. Paul and Pacific Railroad Mainline: St. Anthony to Breckenridge RR Corridor Historic District, no further architectural history work is recommended prior to or during construction for the Willmar Wye project. The St. Paul and Pacific Railroad Mainline: St. Anthony to Breckenridge RR Corridor Historic District is eligible for listing in the National Register; therefore any further work is pending MnDOT's determination of the effects of the project, if any, on this property.

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INTRODUCTION

A public-private partnership including the city of Willmar, Kandiyohi County, the Kandiyohi County and City of Willmar Development Commission, the Minnesota Department of Transportation (MnDOT), and the BNSF Railway Company is proposing a new rail connection between two existing railroad main lines in Willmar, Kandiyohi County, Minnesota, which will require the construction of new crossings at County Road 55, Trunk Highway 12, and Trunk Highway 40. The crossings at the trunk highways will involve vertical and/or horizontal road realignment. This project, the Willmar Connection and Industrial Access Project (Willmar Wye Project), is receiving Department of Transportation funds through the Transportation Investment Generating Economic Recovery (TIGER) competitive grant program, and is therefore subject to review under Section 106 of the National Historic Preservation Act of 1966, as amended. The Federal Railroad Authority (FRA) is the lead federal agency for delivery of physical construction, and the Federal Highway Administration (FHWA) is the lead federal agency for completing the project environmental document, including Section 106 review. FHWA delegates authority for compliance with Section 106 to the MnDOT Cultural Resources Unit (CRU). To assist with their review, the MnDOT CRU contracted with Deco Cultural Services LLC (Deco) to conduct Phase I and II architectural history investigations of the project area of potential effects (APE).

1.1 PROJECT DESCRIPTION

The proposed project is to construct a rail connection between two existing BNSF main track subdivisions, the Morris Subdivision, which parallels Trunk Highway (TH) 12, and the Marshall Subdivision, which parallels County Road (CR) 55/County State Aid Highway (CSAH) 15 (Figure 1). The rail connection will consist of approximately 2.7 miles of new main track, and its configuration will include a single leg on the north, a north-south connection track, and a wye on the south. It is anticipated that the east leg of the wye will provide rail access to the city's industrial park once it is further developed.

A rail siding consisting of approximately 10,000 linear feet of new track will be constructed parallel to the proposed connection on the west to allow trains to pass one another, and an approximately 14-foot-wide access road paralleling and west of the siding will be built to allow BNSF crews to perform train inspections on the connection, to provide access for track and signal maintenance, and to accommodate replacing train crews.

The connection will result in new road crossings at 1st Avenue West/CR 55, TH 40, and TH 12. The road crossing at 1st Avenue West will be an at-grade crossing with active warning devices for crossing protection. The crossing at TH 40 will be a bridge over the new track, and the TH 40/CR 55 intersection located west of the rail line will be raised to meet the TH 40 overpass grade as it descends westward; three properties at this intersection may need to be acquired to allow for the bridge and grade raise. Two alternatives are currently under consideration for the TH 12 crossing, each of which will involve constructing a bridge over the rail line. The first is to follow the current alignment. The second involves re-routing TH 12 to the south

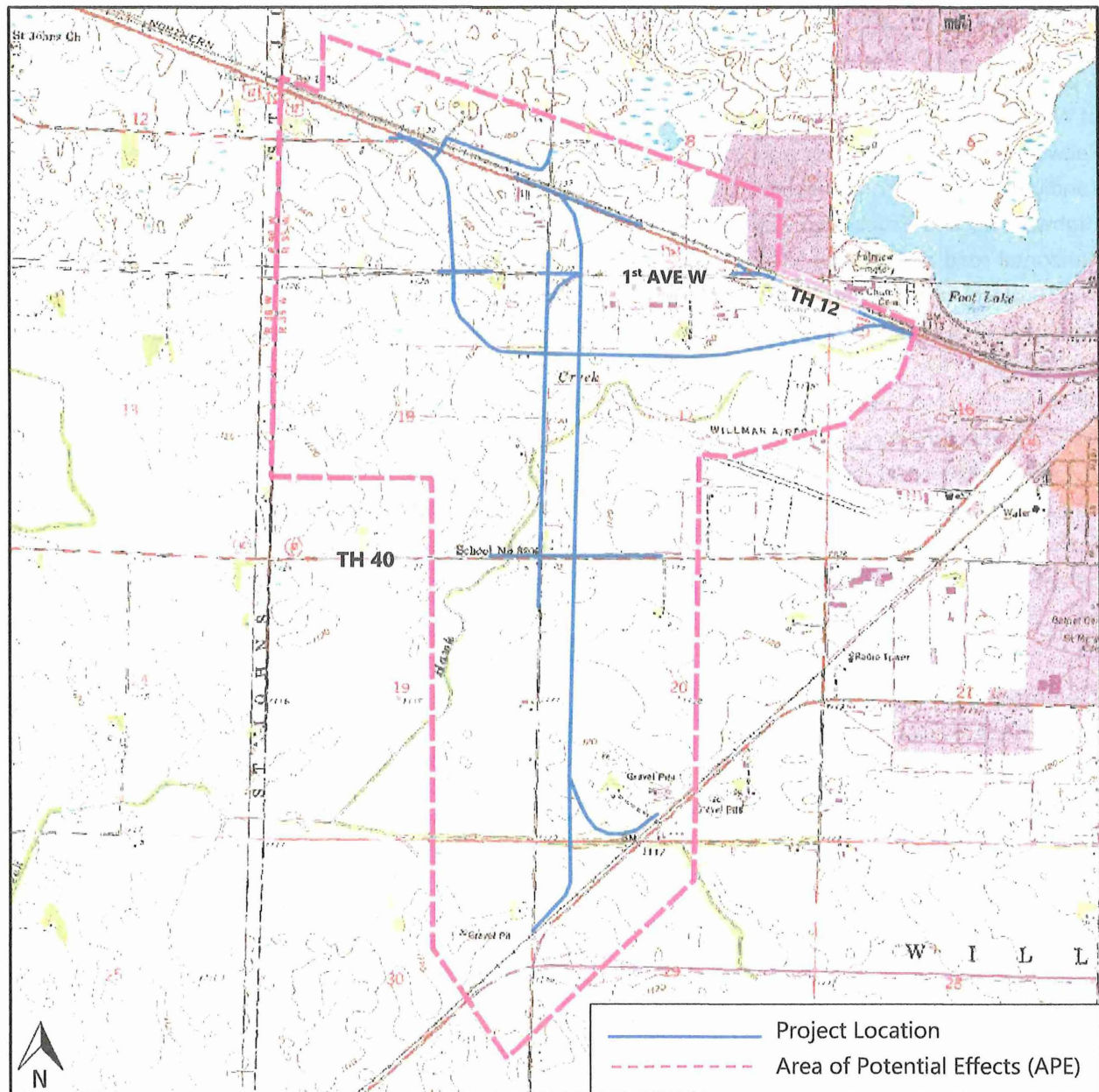
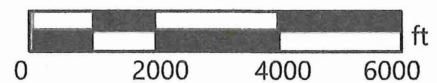


FIGURE 1. PROJECT LOCATION AND AREA OF POTENTIAL EFFECTS (APE)



*(Portions of Pennock, Raymond, Solomon Lake, and Willmar 7.5' USGS Quadrangles,
obtained through MnGeo's Geospatial Image Service)*

to circumnavigate the northern rail-connection point. In addition to road crossings, railway bridges or culverts will be constructed to cross Hawk Creek/County Ditch No. 10, an unnamed tributary to Hawk Creek, and County Ditch No. 46.

1.2 AREA OF POTENTIAL EFFECTS

The APE for architectural history was determined by the MnDOT CRU Project Manager to be an irregularly shaped area which accommodates potential effects for either project alternative (see Figure 1). Legal locations are provided in Table 1. The UTM (NAD 83, Zone 15) coordinates are northernmost point: E 333243 N 5000271, westernmost point: E 332979 N 5000107, easternmost point: E 336640 N 4998689, and southernmost point: E 334183 N 4994493. Coordinates were generated electronically using ACME Mapper 2.1 (<http://mapper.acme.com>).

1.3 STRUCTURE OF THE REPORT

The next chapter describes the methods used in the investigation and is followed by two chapters detailing the results of the Phase I literature review and the field survey. The subsequent two chapters are devoted to the Phase II investigations. The final chapter summarizes the cultural resources management recommendations for the project.

TABLE 1. LEGAL LOCATIONS OF THE APE

Township	Range	Section	Quarter Sections
119	35	7	E-NW-NW, S-NE-NW, S-NW, S-NE, SW, SE
		8	S-SW-NW, SW, NW-NW-SE, S-NW-SE, SW-NE-SE, SW-SE, W-SE-SE
		16	NW-NW, N-SW-NW, SW-SW-NW
		17	NW, NE, SW, W-W-SE, E-NW-SE, N-NE-SE
		18	N-NW, NE, E-SW, SE
		19	NE, SE
		20	NW, W-W-NE, SW, W-W-SE
		29	N-NW, SW-NW, N-SE-NW, SW-SE-NW, W-NW-NE, N-NW-SW, SW-NW-SW
		30	N-NE, N-SW-NE, SE-SW-NW, SE-NE, NE-NW-SE, NE-SE, N-SE-SE

METHODS

The objective of the Phase I architectural history investigation was to identify known historic properties and any properties that are potentially eligible for listing in the National Register of Historic Places (National Register) within the APE. The objective of the Phase II investigation was to determine whether County Ditch No. 10 (KH-DTC-002) and the Willmar and Sioux Falls Railway Company main line (XX-RRD-038) are eligible for listing in the National Register. National Register eligibility is based on the significance criteria outlined below:

- Criterion A – association with events that have made a significant contribution in our past;
- Criterion B – association with the lives of persons significant in our past;
- Criterion C – embodiment of the distinctive characteristics of a type, period, or method of construction; or representation of the work of a master; possession of high artistic values; or representation of a significant and distinguishable entity whose components may lack individual distinction; and
- Criterion D – potential to yield information important to prehistory or history (National Park Service 2002)

In the cases of farm-related properties and railroad-related properties, the application of these criteria was guided, respectively, by the requirements established in *Historic Context of Minnesota Farms, 1820-1960* (Farms Context) (Granger and Kelly 2005) and the National Register Multiple Property Documentation Form titled "Railroads in Minnesota, 1862-1956" (Railroads MPDF) (Schmidt et al. 2007).

All work was conducted per *MnDOT's Cultural Resources Unit Project Requirements* (MnDOT 2011), *Guidelines for History/Architecture Projects in Minnesota*, (Minnesota State Historic Preservation Office [SHPO] 2010), and *The Secretary of the Interior's Standards and Guidelines for Archeology and Historic Preservation* (National Park Service 1983).

2.1 LITERATURE REVIEW

2.1.1 Phase I

A database query request was submitted to the SHPO on January 4, 2016. Once the results were received, relevant files held at the SHPO were reviewed to obtain information on architectural history properties previously inventoried and architectural history surveys previously conducted within the APE. Subsequently, historical fire insurance maps, plat maps, and aerial photographs were consulted to obtain baseline data on construction dates for properties within the APE as available. All of these resources were reviewed to obtain historic contextual information that could be used to frame the potential significance of properties identified during the field survey.

Additional historic contextual information was obtained from other sources held at the Minnesota Historical Society (MNHS) library, the University of Minnesota libraries, and online, primarily histories of Kandiyohi County and the city of Willmar.

2.1.2 Phase II

Following the identification of County Ditch No. 10 and the Willmar and Sioux Falls Railway Company main line as potentially eligible for listing in the National Register, an intensive literature search was conducted to obtain in-depth property histories and information used to develop detailed, relevant historic contexts specific to County Ditch No. 10; such contexts pertaining to the Willmar and Sioux Falls Railway Company main line are contained in the Railroads MPDF. To these ends, Deco reviewed sources such as *The Illustrated History of Kandiyohi County* (Lawson 1905), *The Centennial History of Kandiyohi County* (Kandiyohi County Historical Society [KCHS] 1970), issues of *The Willmar Tribune*, the county and judicial ditch files of the Minnesota Division of Waters, records of the Minnesota State Drainage Commission, *The Great Northern in Minnesota* (Luecke 1997), and the corporate records of the Willmar and Sioux Falls Railway Company. Using the information obtained during the literature review, the Principal Investigator evaluated whether each property met one or more of the four National Register significance criteria.

2.2 FIELD SURVEY

2.2.1 Phase I

A Phase I architectural history survey was performed on March 8-10, 2016. Andrea Pizza served as Principal Investigator and conducted the fieldwork. During the field survey, the Principal Investigator documented all properties 45 years in age or older through field notes and digital photography. Construction dates were estimated based on the literature review and the professional judgment of the Principal Investigator. The potential National Register eligibility of all properties surveyed was assessed under the framework of relevant historic contexts with reference to National Register criteria for significance and integrity.

2.2.2 Phase II

County Ditch No. 10 and the Willmar and Sioux Falls Railroad Company main line were sufficiently examined and documented during the Phase I survey for the purposes of evaluating their National Register eligibility. No additional fieldwork was therefore conducted as part of the Phase II investigation.

PHASE I LITERATURE REVIEW RESULTS

3.1 PREVIOUSLY INVENTORIED PROPERTIES

Two previously inventoried architectural history properties are located partially or entirely within the APE: the St. Paul and Pacific Railroad Mainline: St. Anthony to Breckenridge RR Corridor Historic District (HE-MPC-16387), which has been determined eligible for listing in the National Register, and the Willmar Brick Yard (KH-WLC-072), which has not been evaluated for its National Register eligibility. As recorded in 1985, the Willmar Brick Yard included two kilns, smokestacks, a farmhouse, and outbuildings, all constructed of brick (Minnesota Historic Properties Inventory Form, KH-WLC-072).

3.2 PREVIOUS SURVEYS

No architectural history surveys have been conducted previously within the APE.

3.3 HISTORIC CONTEXTS

The brief historic context that follows is included based on the property types surveyed during the Phase I investigation and provides a framework for the assessment of their potential historical significance. This context is associated with the SHPO statewide historic contexts *Railroads and Agricultural Development, 1870-1940*, and *Railroad Development in Minnesota, 1862-1956* (SHPO 1993; Schmidt et al. 2007). Detailed historic contexts developed to assist in the evaluation of properties during the Phase II investigations are presented with the results of those investigations in subsequent chapters.

3.3.1 Railroads and Agricultural Development of Willmar, 1869-1960s

Although the first Euroamerican settlers arrived in the Willmar area during the late 1850s, any notions they had of permanent habitation were disrupted by the U.S.-Dakota War of 1862; those who survived fled, and the next attempt at putting down roots was not made until the fall of 1866, with the arrival of a Norwegian immigrant named Erick Nelson. Others followed over the course of the next two years, but it was with the platting of the townsite of Willmar by the St. Paul and Pacific Railroad and the quickly ensuing construction of their main line into it in 1869 that a nascent city began to form. The first store was built in that year, and soon other commercial buildings and houses were locating on the blocks proximate to the rail line. In 1871, Willmar was designated as the Kandiyohi County seat, by which time it boasted among its businesses a wide variety of stores and services, a billiard parlor, and three hotels. In 1874, with approximately 1,000 residents to its name, Willmar was incorporated as a village (KCHS 1970:211, 214).

With a St. Paul and Pacific grain elevator constructed there in the summer of 1870, constituting the only one in the vicinity for several years, Willmar became a regional hub for wheat marketing (Lawson 1905:392). After the railroad's monopoly in this regard was ended in the mid 1870s, private interests immediately built more of these structures along the line in Willmar, just as wheat monoculture was shifting from southeastern to western Minnesota, and new flour milling technologies were catalyzing the state's flour manufacturing industry, particularly in Minneapolis, creating a massive demand for the grain (Lawson 1905:401; Granger and Kelly 2005:3.17, 3.19). An 1880 article in Willmar's *Republican Gazette* noted:

We have four elevators and one mil, viz., the Elevator Company's building, capacity 100,000 bushels, J. Lamoine inspector; West End Elevator, E. Locke proprietor, capacity 80,000 bushels; Central Elevator, R. Korthe proprietor, capacity 35,000 bushels; Scandinavian Elevator Company, Bunker & Jacobson, capacity 45,000 bushels. The Meridian Mill, established in 1876, the only one in the immediate vicinity, is 24x42 feet, three stories high, has four rows of burrs, and does a large business. The shipment of wheat from Willmar during the season of 1879 up to [January 29, 1880] was over 400,000 bushels" [quoted in Lawson 1905:401].

With demand for wheat came farmers looking to profit from it, settling on the lands of Willmar and other surrounding townships. Of the 77 heads of household in Willmar Township recorded in the 1880 U.S. Census, 75 were listed as farmers (United States Department of Commerce, Bureau of the Census 1880). Businesses serving both these farmers when they made runs to town and the residents of Willmar proliferated, and the buildings housing them began to be of masonry construction, with a commercial core forming along Pacific and Benson avenues, south of the railroad track (Sanborn-Perris Map Company 1893).

Although a shift from wheat monoculture began to occur circa 1885, wheat remained a major focus of Minnesota farmers, particularly in the western part of the state, and therefore cause for competition among railroads. By this time, the St. Paul Minneapolis and Manitoba Railway Company (Manitoba), under the management of James J. Hill, had purchased the St. Paul and Pacific. Willmar, with a railroad yard and grain processing facilities already in place, became a logical location through which the Manitoba would run as Hill looked to win this competition in southwestern Minnesota. It came to pass, therefore, that in 1885-1886, the Manitoba-backed St. Cloud Mankato and Austin Railroad Company constructed a line from St. Cloud to Willmar, and then in 1887, construction began in Willmar on the Manitoba-backed Willmar and Sioux Falls Railway Company line. The combination not only provided access to southwestern wheat, but at nearly the time that the Willmar and Sioux Falls line reached Sioux Falls in November of 1888, a through-route was created from Sioux Falls, South Dakota, to Lake Superior, via a line of the Eastern Railway Company of Minnesota, another Hill road, which was completed in the same month (Luecke 1997:72, 116-117, 120, 129). Willmar had the enviable position of being a division point for the Manitoba and later, the Great Northern Railway Company (Great Northern), with direct connections to Lake Superior, the Twin Cities, the Red River Valley, Sioux Falls, and intervening agricultural lands. Perhaps not coincidentally, the first three of Kandiyohi County's ditches were established in Willmar Township in 1885 and 1886, as farmers sought to improve the productivity of their lands (Lawson 1905:387-388).

The railroads would support a wide variety of industrial endeavors in Willmar, not all of which were related to agriculture. The first brickyard was established just a few blocks south of the railroad in 1888, and the company, under the ownership of John Dehlbom and others, would locate along the Manitoba/Great Northern main line three years later. Best known as the Willmar Brick Company, this company would successfully operate in Willmar until 1950. Non-agricultural industries in Willmar between 1905 and 1940 included a gasoline works, cuspidor plant, a foundry, boiler companies, a strawboard manufacturer, a glove factory, a wheel and tire company, cigar factories, bottling works, cement-related industries, sand and gravel

companies, sash and door companies, a paint factory, a mattress works, and a wood working operation (KCHS 1970:285). On the agricultural side were operations such as the grist mill built in 1896 by William Peterson, and in the next few decades, those of companies such as the Willmar Creamery Company, Willmar Tractor Manufacturing Company, Minnesota Spreader Company, Central Dairy Products Company, International Milling Company, and Allstate Hatchery (KCHS 1970:285, 268-269). Allstate, who introduced turkeys into their operations in 1930, was the forerunner in what would become the most important component of Willmar's post-World War II agricultural economy, the turkey industry, including both breeding and processing (KCHS 1970:268).

Two major interests engaged in the turkey industry emerged in the post-war era: the Willmar Poultry Company and the Farmers Produce Company. The Willmar Poultry Company was formed by H. I. Nelson, Albert Husinga, Fred and Albin Norling, and Charles Bischoff in 1945, the latter exiting the venture the following year. Initially engaged only in turkey processing, the company added hatchery operations within just a few years, and then in 1953 eliminated processing to focus on the hatcheries. The company subsequently established additional hatcheries in Redwood Falls (1954), Litchfield (1960), and other locations in Minnesota. Beginning in the late 1950s, the company established farms to handle overages in the poult population stemming from order cancellations at the hatcheries. Thirteen such farms existed by 1970, by which time the Willmar Poultry Company was the "largest turkey hatchery in the world" (KCHS 1970:268), having an egg capacity of 1.75 million per month. As such, it contributed to Kandiyohi County's status as the top turkey producer in the nation.

On the processing end, beginning in 1949, was the Farmers Produce Company. What started in that year under the ownership of Earl B. Olson as a single turkey processing plant with 60 employees became within two decades a large-scale enterprise with more than 1,000 employees processing five million turkeys a year in Willmar, Litchfield, and Melrose. In Willmar, operations were augmented between 1963 and 1969 with "twenty farms raising over two million head of turkey; three feed mills, a research farm, and a feather processing plant" (KCHS 1970:267). The Farmers Produce Company supplied an international market for the bird and was the corporation behind the brands Jennie-O, named after Olson's daughter, and Festive. Its Further Processing Division "developed . . . fourteen major products, such as turkey pan roasts, oven roasted breasts, [and] cooked turkey rolls . . . sold to over-the-counter trade and to institutional operations" (KCHS 1970:267).

The turkey industry remains a significant part of the Willmar economy, with both Jennie-O, now known as Jennie-O Turkey Store and owned by Hormel Foods Corporation, and the Willmar Poultry Company, now "the key division of a privately held turkey industry empire in Willmar called Life-Science Innovations" (Hughlett 2015), maintaining headquarters and large-scale operations there (Jennie-O Turkey Store 2016). The transportation needs of these and other industries continue to be serviced in no small part by the railroads that run through the city, which are now part of the BNSF system.

PHASE I FIELD SURVEY RESULTS

The Phase I architectural history investigation for the Willmar Wye Project identified 27 architectural history properties within the APE. Of these, one property, the St. Paul and Pacific Railroad Mainline: St. Anthony to Breckenridge RR Corridor Historic District (HE-MPC-16387), previously was determined eligible for listing in the National Register (Table 1). The properties surveyed include 15 current or former farmsteads, 6 houses, 2 current or former industrial facilities, 2 county ditches, and 1 railroad, (Figure 2), two of which are potentially eligible for listing in the National Register: County Ditch No. 10 (KH-DTC-002) and the Willmar and Sioux Falls Railway Company main line (XX-RRD-038).

TABLE 2. SUMMARY OF PHASE I RESULTS

Inventory No.	Property Name	Property Address	Recommendation
HE-MPC-16387	St. Paul and Pacific Railroad Mainline: St. Anthony to Breckenridge RR Corridor Historic District		Previously found eligible
KH-WLC-072	Willmar Brick Yard	South of Highway 12, west of 30 th Street NW	Not eligible
KH-WLT-001	Farmstead	5090 Highway 12 NW	Not eligible
KH-WLT-002	Farmstead	790 45 th Street NW	Not eligible
KH-WLT-003	Farmstead	699 45 th Street NW	Not eligible
KH-WLT-004	Farmstead	5840 7 th Avenue NW	Not eligible
KH-WLT-005	Farmstead	5812 7 th Avenue NW	Not eligible
KH-WLT-006	Farmstead	5591 7 th Avenue NW	Not eligible
KH-WLT-007	Diamond Farm	4611 Highway 12 NW	Not eligible
KH-WLT-008	Farmstead	5550 1 st Avenue W	Not eligible
KH-WLT-009	Farmstead	4746/4747 1 st Avenue W	Not eligible
KH-WLT-010	Farmstead	1399 45 th Street SW	Not eligible
KH-WLT-011	Farmstead	1560 45 th Street SW	Not eligible
KH-WLT-012	Farmstead	2280 45 th Street SW	Not eligible
KH-WLT-013	House	2460 45 th Street SW	Not eligible
KH-WLT-014	Barn	45 th Street SW, north of BNSF line	Not eligible
KH-WLT-015	Farmstead	3830 County Road 15 SW	Not eligible
KH-WLT-016	Farmstead	3843 15 th Avenue SW	Not eligible
KH-WLT-017		3631/3637 15 th Avenue SW	Not eligible
KH-WLC-135	House	3055 Highway 12 W	Not eligible
KH-WLC-136	House	101 33 rd Street NW	Not eligible
KH-WLC-137	House	103 33 rd Street NW	Not eligible
KH-WLC-138	House	105 33 rd Street NW	Not eligible
KH-WLC-139	House	107 33 rd Street NW	Not eligible
KH-DTC-001	County Ditch No. 46		Not eligible
KH-DTC-002	County Ditch No. 10		Potentially eligible
XX-RRD-038	Willmar and Sioux Falls Railway Company main line		Potentially eligible

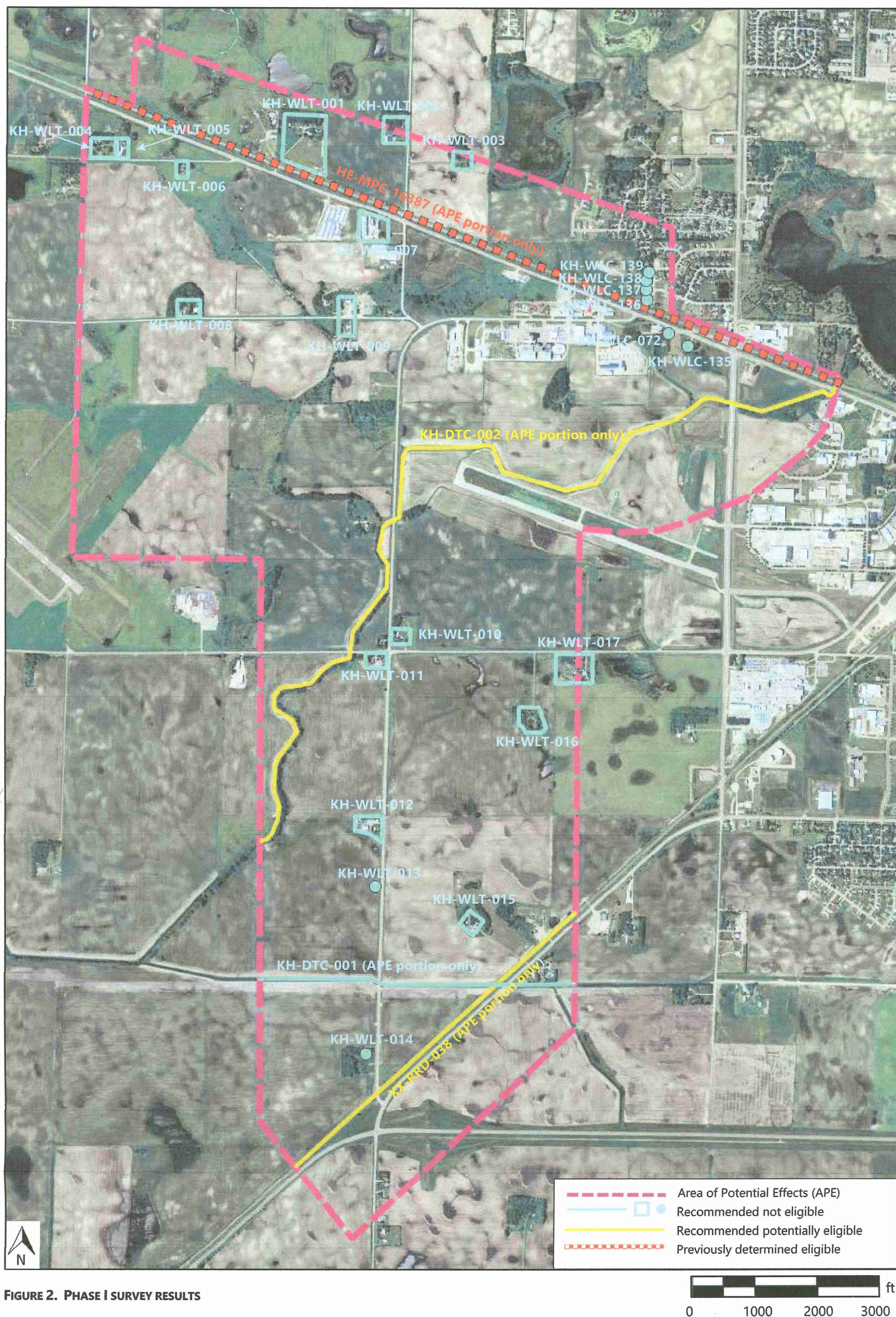


FIGURE 2. PHASE I SURVEY RESULTS

The property histories provided for the farmsteads in the following sections are based on available plat maps, which date to 1886 (North West Publishing Company), 1915 (Webb Publishing Company), 1925 (W. W. Hixson & Company), 1932 (Webb Publishing Company), 1954 (Thomas O. Nelson Company), 1957 (Farm Plat Book Publishing Company), and 1961 (Thomas O. Nelson Company), as well as aerial photographs dating to 1938, 1955, 1963, 1970, and the modern era.

4.1 HE-MPC-16387

St. Paul and Pacific Railroad Mainline: St. Anthony to Breckenridge RR Corridor Historic District Adjacent/Parallel to TH 12

This railroad was constructed in 1862 and 1867 to 1871 by the St. Paul and Pacific Railroad as its main line from St. Paul to Breckenridge, reaching Willmar in 1869. The St. Paul and Pacific Railroad was purchased by the St. Paul Minneapolis and Manitoba Railway Company in 1879, whose properties were in turn leased and then acquired in 1890 and 1907, respectively, by the Great Northern. Under the auspices of the Great Northern, the main line reached Seattle in 1893, becoming the country's fifth and northernmost transcontinental railroad line.

In the project APE, the railroad corridor consists of a railroad roadway, which begins on the east as a double track incorporating wood ties and steel rails supported by a slightly raised rail bed before merging into a single track of the same materials approximately 0.2 mile east of 45th Street NW (Figure 3). The railroad line remains active as part of the BNSF system.



FIGURE 3. ST. PAUL AND PACIFIC RAILROAD MAINLINE AT 45TH STREET NW, LOOKING SE

4.1.1 Recommendations

This railroad corridor historic district was previously determined to be eligible for listing in the National Register.

4.2 KH-WLC-072

Willmar Brick Yard

South of TH 12, west of 30th Street NW

In 1892, the Willmar Brick Company began operations in the area south of TH 12 and west of 30th Street NW as Dehlbom, Johnson & Co., who had moved there after exhausting their supply of clay in another Willmar location. Dehlbom, Johnson & Co. was renamed the Willmar Brick Company in 1899 after Nels Flykt bought out the shares of part-owner John Dehlbom (Lawson 1905:388). Bricks, along with the later addition of clay tile, were manufactured at the site under various company names under Nels Flykt and then his son, Clarence, until 1950 (KCHS 1970:285).

When the Willmar Brick Yard was inventoried in 1985, two kilns, brick smokestacks, a brick farmhouse, and other outbuildings associated with the operations of the Willmar Brick Company were present on the site (Minnesota Historic Properties Inventory Form, KH-WLC-072, on file at the SHPO). Since that time, nearly all of the structures and buildings associated with the Willmar Brick Company have been removed, leaving a single brick smokestack (Figure 4).



FIGURE 4. WILLMAR BRICK YARD SMOKESTACK, LOOKING SE

4.2.1 Recommendations

As an appurtenance to a former kiln, the smokestack does not meet the definition of and therefore would not be evaluated as a masonry ruin under the National Register Multiple Property Documentation Form titled "Minnesota's Nineteenth-Century Masonry Ruins" (Pizza 2015:F2). Although the Willmar Brick Yard may hold significance under Criterion A within the context of Railroads and Agricultural Development in Willmar, 1869-1960s, it does not have the integrity to convey any potential significance in this regard, due to the loss of the all but one of its buildings and structures. No evidence could be located to suggest that any of the owners of the Willmar Brick Company were historically important in the brickmaking industry, but if they were, the smokestack alone would not be sufficient to convey the significance of any of these individuals under Criterion B. The smokestack, which is basic in its construction and does not incorporate any innovation in its design, does not embody the distinctive characteristics of a type, period, or method of construction; represent the work of a master; possess high artistic values; or represent a significant and distinguishable entity whose components may lack individual distinction. It therefore does not individually meet Criterion C. The Willmar Brick Yard is therefore recommended as not eligible for listing in the National Register.

4.3 KH-WLT-001

Farmstead

5090 Highway 12 NW

Based on plat maps and historical aerial photographs, this farmstead was established sometime between 1932 and 1938. A plat map dating to the former year lists the owner as Frank R. Zwemke. In 1954, the property was owned by Wallace and Beryl Zwemke, who retained it through at least 1957.

The farmstead includes two houses, a garage, two granaries, a chicken coop, a barn, a silo, a machine shed, a Quonset building, several grain bins, and five additional outbuildings. One of the houses is a large, modern house (Figure 5). The second is a circa-1935, one-story, side-gabled house with a concrete-block foundation. It has been altered with a flat-roofed addition on the north end, fiberboard siding, and replacement windows and doors (Figure 6). Three other buildings and structures along the north edge of the farmstead appear to have been constructed around the same time as this earlier house: a garage, a granary, and a chicken coop (Figures 7 to 9). The single-bay garage has a gabled roof, covered in asphalt shingles, with exposed rafter tails. The walls are wood drop siding, and an original four-light fixed-sash window is located on the rear of the building. The superstructure is raised on wood supports, which either were placed as a measure against deterioration or may indicate that this building was moved from another location.

The granary is elevated on concrete skids. It has a gabled roof clad in wood shakes, and a four-light, fixed-sash window is located in the west gable end. Shed wings are located on the east end and the north side, both of which are accessed by sliding doors, the door to the north shed wing located on the west side of the building, and the door to the east shed wing located on the south side of the building. To the west of



FIGURE 5. 5090 HIGHWAY 12 NW, MODERN HOUSE, LOOKING NE



FIGURE 6. 5090 HIGHWAY 12 NW, CIRCA-1935 HOUSE, LOOKING NE



FIGURE 7. 5090 HIGHWAY 12 NW, GARAGE, LOOKING NW



FIGURE 8. 5090 HIGHWAY 12 NW, GRANARY, LOOKING NW

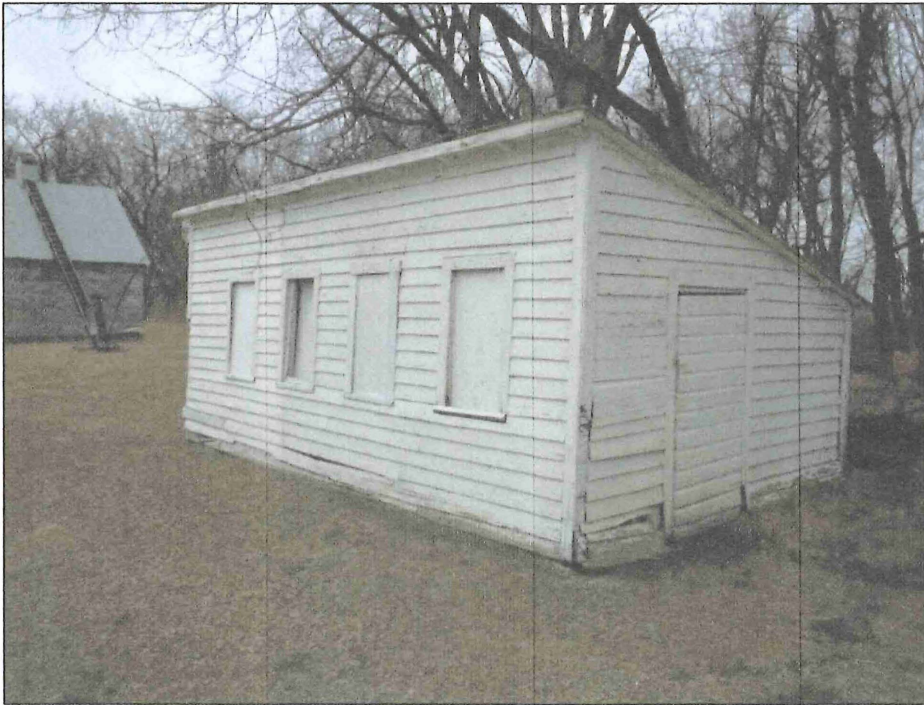


FIGURE 9. 5090 HIGHWAY 12 NW, CHICKEN COOP, LOOKING NW

the latter sliding door is a pedestrian door that provides entry to the main portion of the granary, and to the west of the pedestrian door is a small hatch door. The entirety of the building is clad in wood drop siding. The chicken coop, which is west of the granary, has a shed roof with exposed rafter tails, and the roof is covered in corrugated metal. The walls are wood lap siding. Four window openings are located on the south side, all of which have been infilled with plywood. A pedestrian door is on the east end.

Later additions to the farmstead are interspersed with these buildings along the north edge of the farmstead. To the east of the garage is a circa-1950 Quonset building on a concrete-block foundation (Figure 10). Its original wood drop siding is evident on the south (front) and north ends of the building, but the arched portion has been covered with roll roofing. Fenestration includes both original four-light, fixed-sash windows and replacement windows in the north and south walls. Rough cuts indicate these walls have been altered to create larger openings for non-original doors that allow for drive-through capacity.

On the west side of the garage is a machine shed covered in corrugated metal sheeting (Figure 11). Aerial photos show a building was placed in this location circa 1960, but any original details of the machine shed are obscured by its cladding. The obscuring of details similarly applies to a later granary with portable elevator located west of the chicken coop (Figure 12). This gable-roofed building is almost entirely covered in sheet metal panels, but original wood drop siding is visible on the north (rear) gable. Exposed rafter tails are visible on both ends of the building. One original six-light, fixed-sash window is present in the south gable, but a second window opening in the same gable is infilled with sheet metal. These visible similarities



FIGURE 10. 5090 HIGHWAY 12 NW, QUONSET BUILDING, LOOKING NW



FIGURE 11. 5090 HIGHWAY 12 NW, MACHINE SHED, LOOKING NW



FIGURE 12. 5090 HIGHWAY 12 NW, GRANARY WITH PORTABLE ELEVATOR, LOOKING NW

to the circa-1935 buildings suggest that this building may have been constructed around the same time, but if so, a 1938 aerial photograph indicates that it was moved to this location at a later date. The granary is raised on concrete skids, and it features a sliding door of vertical wood planking on both ends.

West of the granary are two heavily altered outbuildings that were put into place sometime between 1938 and 1955. The first incorporates original concrete-block walls or portions thereof (Figure 13). A gable evident over the concrete-block portion appears to constitute a later superstructure, and still later was an expansion of the building to the east, nearly doubling its size. The expanded portion and the gables above the concrete-block portion have been covered with wood drop siding, into which cuts were made to create or re-create loft doors. Original fenestration appears to be limited to the six-light fixed sash in the concrete-block portion. The roof has been covered with corrugated metal. The second outbuilding, a barn, has had vertical wood planking added to the north and south ends, which was roughly cut to create doorways (Figure 14). The west side and the roof have been clad in corrugated metal, and the east side has a slatted ventilating wall of horizontal wood planks. A pulley extends from an opening in the mow. To the northwest of the barn is a simple post and beam loafing shed (Figure 15), and to the south is a cement-stave silo (Figure 16). A third outbuilding put up during the period between 1938 and 1955 is located south of the chicken coop (Figure 17). Beyond a small exposure of horizontal wood plank siding, no original architectural details are visible on this gable-roofed building because it has been clad entirely in corrugated metal, except where openings exhibit replacement or added window and doors.



FIGURE 13. 5090 HIGHWAY 12 NW, OUTBUILDING, LOOKING S-SE



FIGURE 14. 5090 HIGHWAY 12 NW, BARN, LOOKING NE



FIGURE 15. 5090 HIGHWAY 12 NW, LOAFING SHED, LOOKING NW

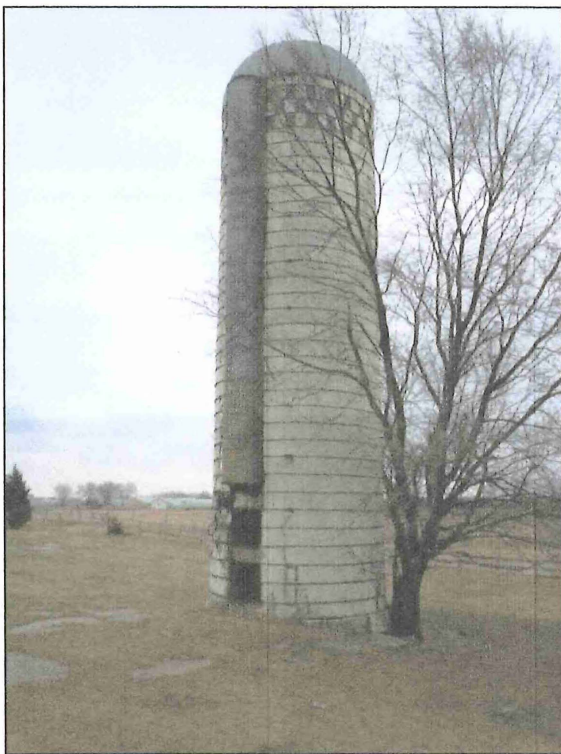


FIGURE 16. 5090 HIGHWAY 12 NW, SILO, LOOKING SE



FIGURE 17. 5090 HIGHWAY 12 NW, OUTBUILDING, LOOKING NW

Aerial photographs indicate that additional outbuildings and structures present on the farmstead post-date the period of significance for Minnesota farmsteads. These include several steel grain bins (Figure 18) and a large livestock building (Figure 19) put up on the property during the mid to late 1960s, and two other livestock buildings constructed after 1970 (Figures 20 to 21). The grain bins are located to the north of the circa-1935 house, while the livestock buildings are concentrated in the southeast portion of the farmstead.

4.3.1 Recommendations

Based on the extensive, and in some cases severe, alterations to the buildings and structures on this property, combined with the conspicuous presence of the modern house, this farmstead would not have the integrity to convey any potential historical significance under National Register Criterion A, B, or C. None of the individual elements of the farmstead are especially distinctive examples of architecture or design as required by the Farms Context for significance under Criterion C (Granger and Kelly 2005:7.20). This property is therefore recommended as not eligible for listing in the National Register.



FIGURE 18. 5090 HIGHWAY 12 NW, GRAIN BINS, LOOKING N

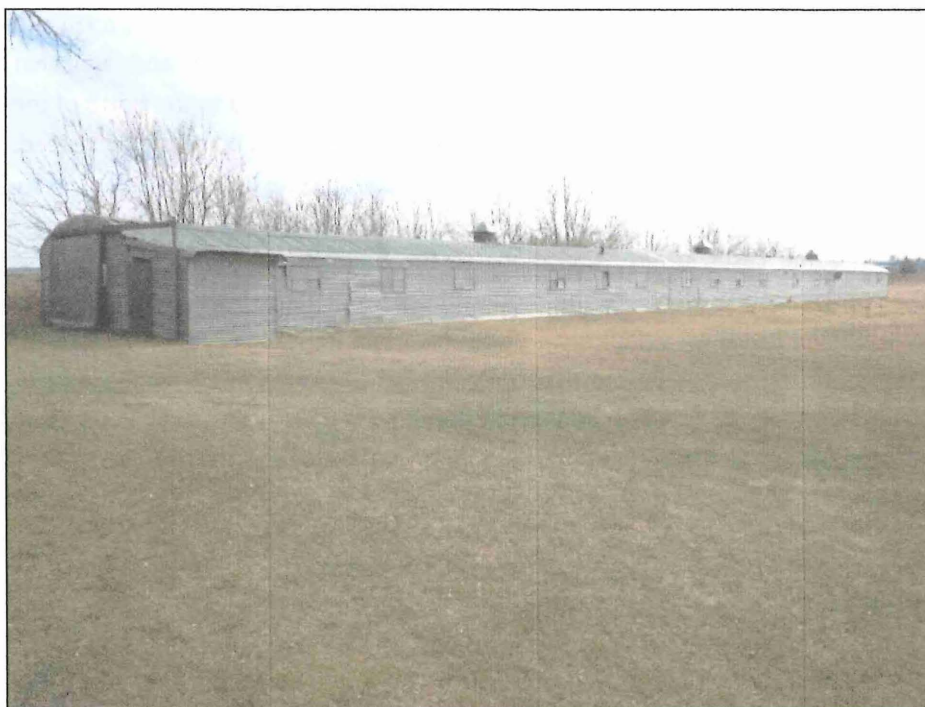


FIGURE 19. 5090 HIGHWAY 12 NW, LIVESTOCK BUILDING, LOOKING SE

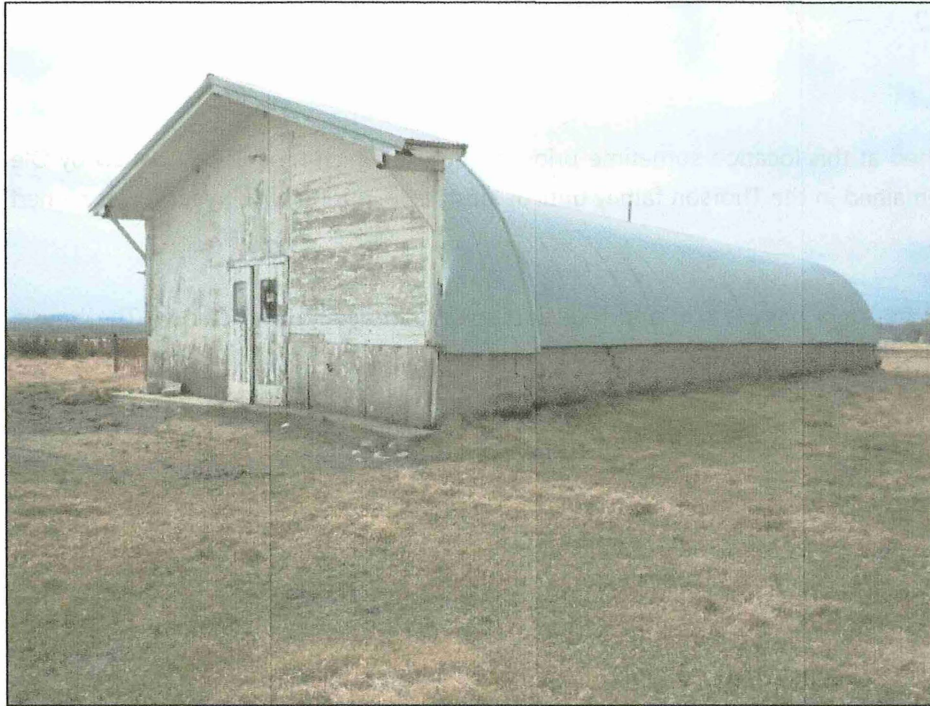


FIGURE 20. 5090 HIGHWAY 12 NW, LIVESTOCK BUILDING, LOOKING SW



FIGURE 21. 5090 HIGHWAY 12 NW, LIVESTOCK BUILDING, LOOKING SE

4.4 KH-WLT-002

Farmstead

790 45th Street NW

A farmstead was established at this location sometime prior to 1886, at which time it was owned by Ole Thorson. The property remained in the Thorson family until at least 1957, during which year it was owned by Oscar Thorson.

This farmstead includes a house, a barn, a silo, and four additional outbuildings/structures. The house, which appears to have been built as a one-and-a-half-story house with gabled roof has been extensively modified through several additions, including an attached garage and adjoining extension of the house on the south, addition on the east with added bay window, and a sunporch inset in the resultant ell; the installation of vinyl siding and soffits; and the replacement of windows and doors, such that a construction date cannot be discerned (Figure 22).

Set somewhat behind the house to the west-southwest is a small, one-story storage building having a low-pitched roof, with walls and roof clad in corrugated metal (Figure 23). Its foundation is not visible. Fenestration appears original, with paired 1/1 double-hung sash windows on the east and two nine-light, fixed-sash windows on the west. What appears to be original wood framing with crown molding surrounds the windows, as well as the single-panel wood pedestrian door on the south end.

To the south of the storage building and house is a driveway that divides them from the remaining outbuildings and structures. The barn, or former barn, is banked on the north, where an original concrete-faced, mortared fieldstone foundation incorporated into the current building is evident (Figure 24). Beyond this exposure on the north edge, what is assuredly a replacement superstructure is clad in corrugated metal, as is the roof. A larger, post-1963 addition to the building extends in a perpendicular fashion from its south side. Sliding doors are present on the east and west sides of this extension, and a pedestrian door is located on the east side of the extension as well. No windows are present on the building. North of and adjacent to the banked edge is a remnant cement-stave silo (Figure 25).

A circa-1960 corrugated metal-clad Quonset building with sliding doors is located northwest of the barn (Figure 26), and a circa-1965 outbuilding, which was subsequently expanded, is to its west (Figure 27). The latter outbuilding also is clad in corrugated metal, with cuts made to accommodate an overhead door and a pedestrian door on the north side, which additionally features a sliding door. A second sliding door is located on the south side.



FIGURE 22. 790 45TH STREET NW, HOUSE, LOOKING NE

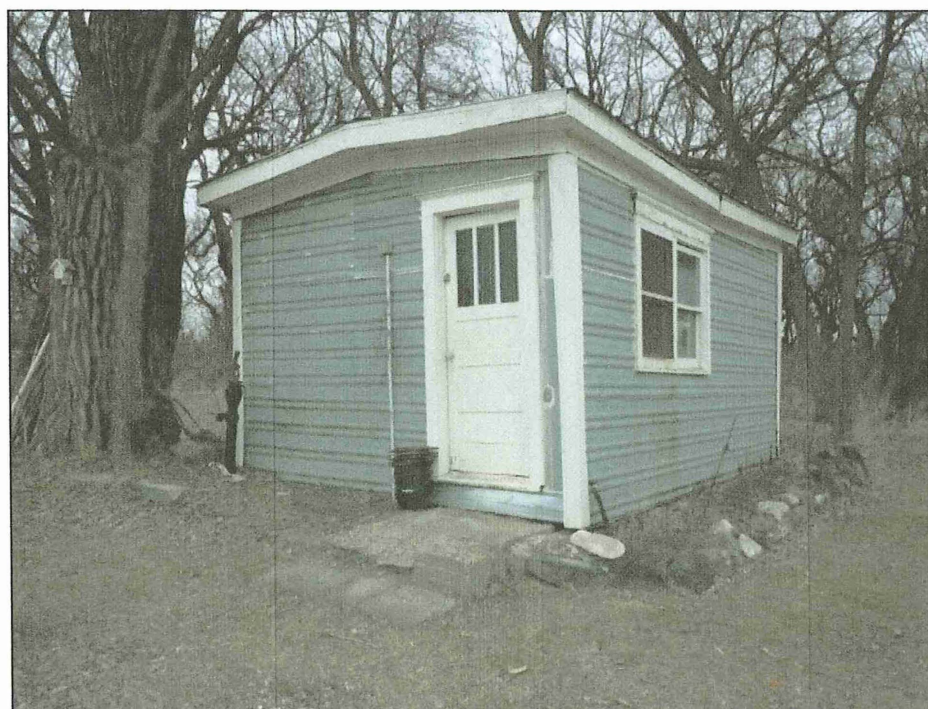


FIGURE 23. 790 45TH STREET NW, STORAGE BUILDING, LOOKING NW



FIGURE 24. 790 45TH STREET NW, BARN WITH ADDITION IN FOREFRONT, LOOKING NE

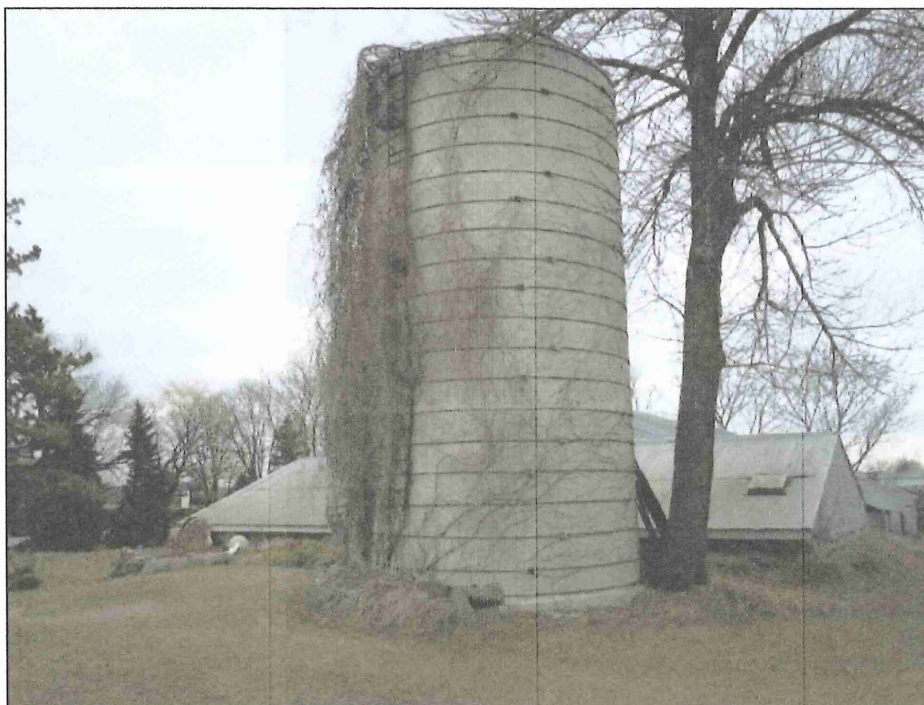


FIGURE 25. 790 45TH STREET NW, SILO, LOOKING SE

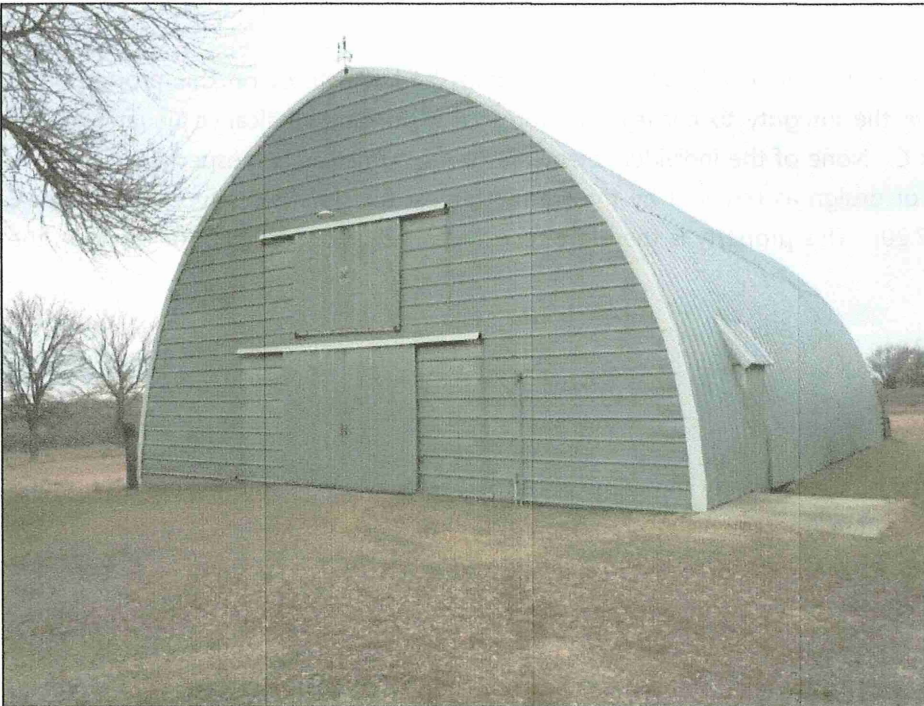


FIGURE 26. 790 45TH STREET NW, QUONSET BUILDING, LOOKING SW



FIGURE 27. 790 45TH STREET NW, OUTBUILDING, LOOKING SE

4.4.1 Recommendations

Based on the extensive alterations to nearly all of the buildings and structures on this property, this farmstead would not have the integrity to convey any potential historical significance under National Register Criterion A, B, or C. None of the individual elements of the farmstead are especially distinctive examples of architecture or design as required by the Farms Context for significance under Criterion C (Granger and Kelly 2005:7.20). This property is therefore recommended as not eligible for listing in the National Register.

4.5 KH-WLT-003

Farmstead

699 45th Street SW

A farmstead was established on this property sometime prior to 1886, in which year it was owned by John Rasmusson. The Rasmusson family retained the property through at least 1932, when it was owned by Benhardt Rasmusson. A property owner is not noted for 1954, but from 1957 through at least 1961, the farmstead was owned by Harry and Thelma Solberg.

This former farmstead includes a house, barn, remnant silo, possible milk house, machine shed, granary, privy, and one other outbuilding. The circa-1880, one-and-a-half-story, front-gable-and-wing house has a poured-concrete foundation (Figure 28). With the exception of a small shed-roofed addition, the walls are clad in wood lap siding. All gables contain a decorative sunray motif underlined by the crown molding that is over the upper windows. Extending down the corners of the house from the ends of each gable are wood pilasters that mimic columns (Figure 29). A deck-roof enclosed porch is in the ell on the north side of the house; from its west side extends the previously mentioned small shed-roofed addition, which is clad in vertical wood-plank siding and contains a newer door on the west. The deck-roofed enclosed porch either is contemporaneous with the house or was enclosed not too long after its construction and represents a conscientious effort to create consistency in its appearance. It features the same wood lap siding, pilasters, and window style. Windows on the main house and the porch are largely original 1/1 double-hung sash with newer storm windows surrounded by wood frames with crown molding. The window, however, on the lower story of the west end of the house has been altered and contains a 1/1 fixed sash over an inset row of five glass blocks. It exhibits a simple, narrow wood frame with no crown molding. An original wood door is located on the south side of the wing. The roof is covered in asphalt shingles, and original wood soffits and fascia remain intact. An interior brick chimney projects above the peak of the wing.

The barn is located on the south edge of the farmstead just above a field edge. It is a circa-1920, gothic-arch, bank barn flanked on both sides by attached wings (Figure 30). The east wing was constructed circa 1965 and has a concrete-block foundation that supports a side-gabled superstructure clad in wood drop siding on the south and in horizontal wood planking on the south and east (Figure 31). Within the foundation on the south side is a central machine-door opening, to the west of which is a wood pedestrian door. These are flanked on both sides by a single glass-block window. In the upper level are regularly



FIGURE 28. 699 45TH STREET NW, HOUSE, LOOKING N-NW



FIGURE 29. 699 45TH STREET NW, HOUSE, LOOKING SW

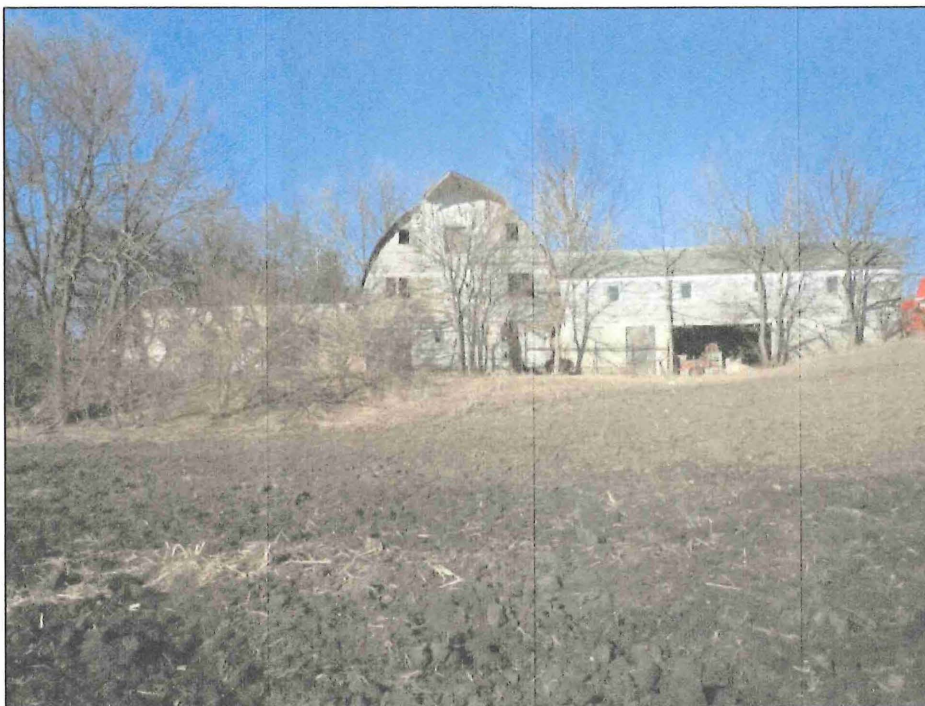


FIGURE 30. 699 45TH STREET NW, BARN, LOOKING N



FIGURE 31. 699 45TH STREET NW, EAST WING OF BARN, LOOKING N

spaced 1/1, fixed-sash windows. A pedestrian door opening is present on the east end, while the north side contains two window similar to those on the south. The roof is clad in asphalt shingles. The west wing was constructed at an unknown date (Figure 32). Although a west wing appears on a 1938 aerial photograph of the property, the current truncation of a stone wall near the junction of the barn and wing by a poured-concrete foundation and a superstructure of horizontal wood-plank siding suggest the construction currently present dates to a later period. This wing, which has a flat roof, features regularly spaced openings for four-light, fixed-sash windows, some of which are missing, and a vertical wood-plank pedestrian door on the south side. The rear of this wing is built into the hillside.

The original, main portion of the barn is clad in wood drop siding (Figure 33). The south end faces out toward agricultural fields and features a hay hood over the mow door. The mow door is flanked on each side by a small window opening, beneath which are larger window openings, none of which contain windows. These openings are mirrored on the north side of the barn, where the larger window openings contain paired six-light fixed-sash windows. Beneath the larger window openings on the south side at ground level are two sliding doors, the eastern one of which has deteriorated. Between these doors are three window openings, one of which no longer contains a window, and the other two of which hold original four-light, fixed-sash windows. On the east side of the main barn, a pedestrian door is located at the level of the bank. The barn roof is covered in asphalt shingles.

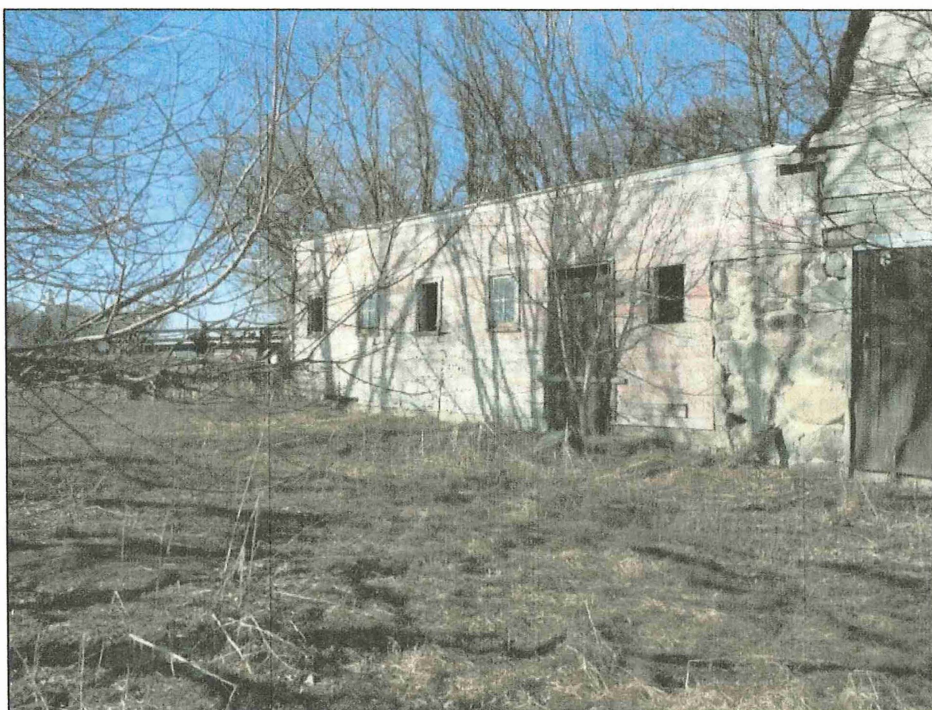


FIGURE 32. 699 45TH STREET NW, WEST WING OF BARN, LOOKING NW

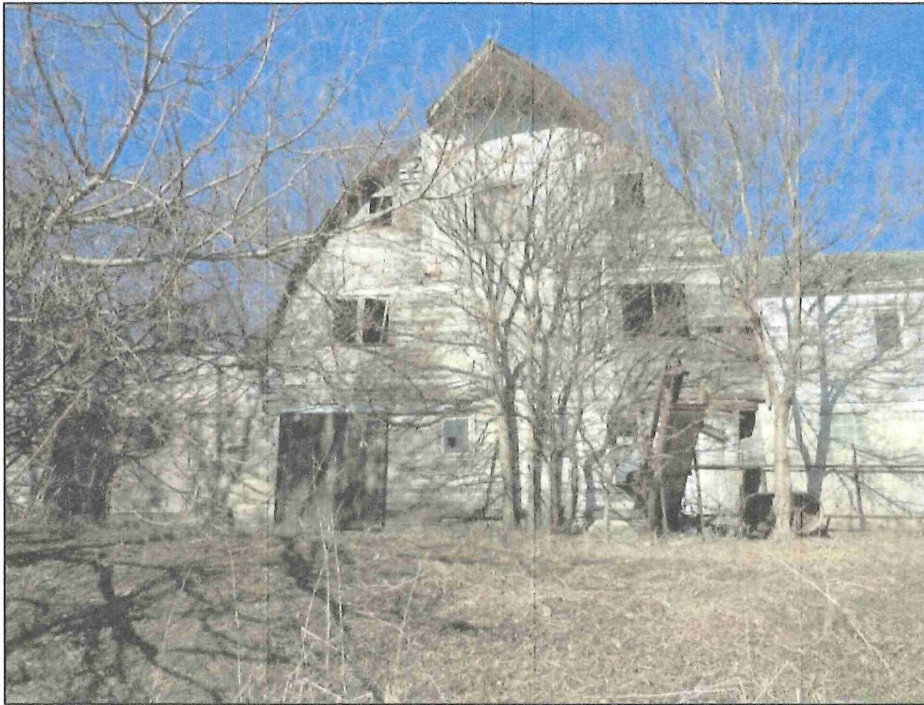


FIGURE 33. 699 45TH STREET NW, ORIGINAL BARN, LOOKING N

Near the northeast end of the gothic-arch portion of the barn is the base of a cement-stave silo (Figure 34). The small area between the barn and silo was not accessible and was therefore not visible as it is enclosed on the east side by a horizontal wood-plank wall containing a vertical wood-plank pedestrian door and on the west side by a small, concrete-block structure built into the hillside and having a pedestrian door on the north side, possibly a milk house (Figure 35). Lying on the east side of the silo is the overturned superstructure of a small, frame outbuilding with wood drop siding (Figure 36).

To the east-northeast of the barn is an outbuilding, possibly an earlier granary, in a highly deteriorated state (Figure 37). It is a gabled with a shed wing and sided with horizontal wood planking. Only the west wall was observable during the survey; a door opening and one window opening are located in this wall, but the door and window are no longer present. The roof is covered with asphalt shingles. Just northwest of this outbuilding is a gable-roofed granary, which is raised on concrete skids and blocks (Figure 38). The walls are clad in wood drop siding, and it retains original four-light, fixed-sash windows in the west gable and under the eave on the south side. A deteriorated, vertical wood-plank pedestrian door is located on the north side. The roof is covered in asphalt shingles.

North-northwest of the barn on the north side of the driveway is a side-gabled machine shed with three south-facing bays, each of which holds an overhead door of vertical wood planking (Figure 39). Although this building appears to date to the 1920s or 1930s, aerial photographs indicate that it was not in its current location until sometime after 1963, so it was likely moved from elsewhere on the farmstead. A pedestrian

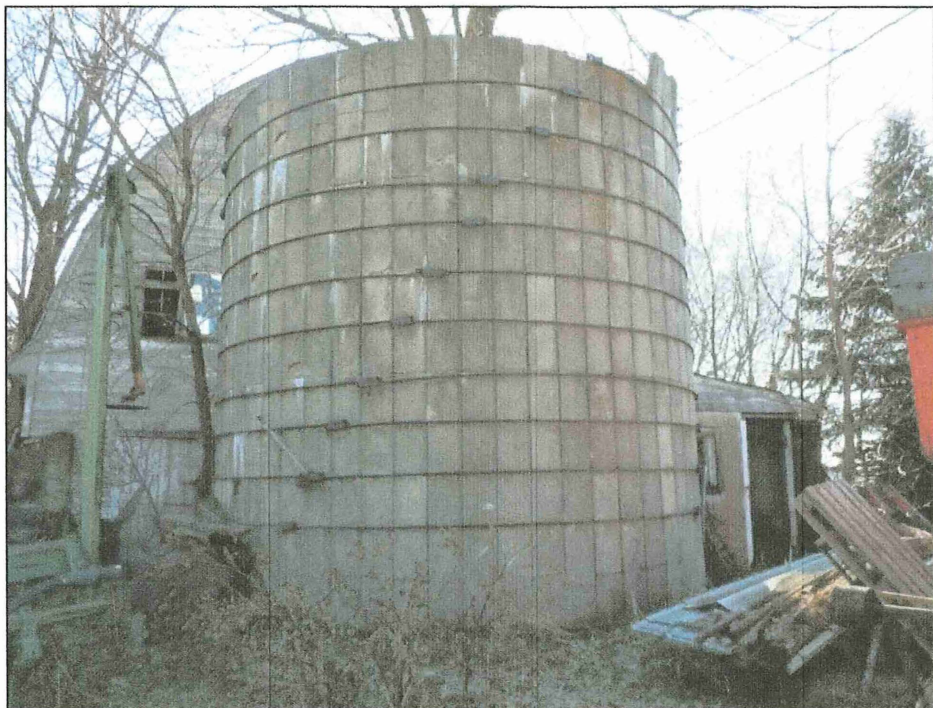


FIGURE 34. 699 45TH STREET NW, SILO, LOOKING S-SW

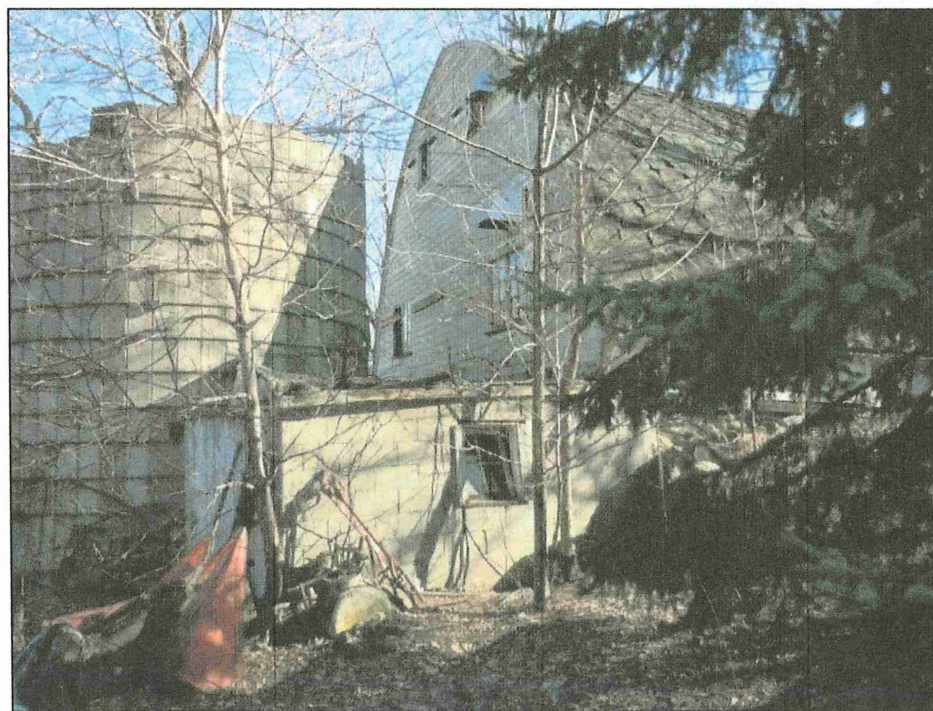


FIGURE 35. 699 45TH STREET NW, POSSIBLE MILK HOUSE, LOOKING E



FIGURE 36. 699 45TH STREET NW, COLLAPSED OUTBUILDING, LOOKING SW

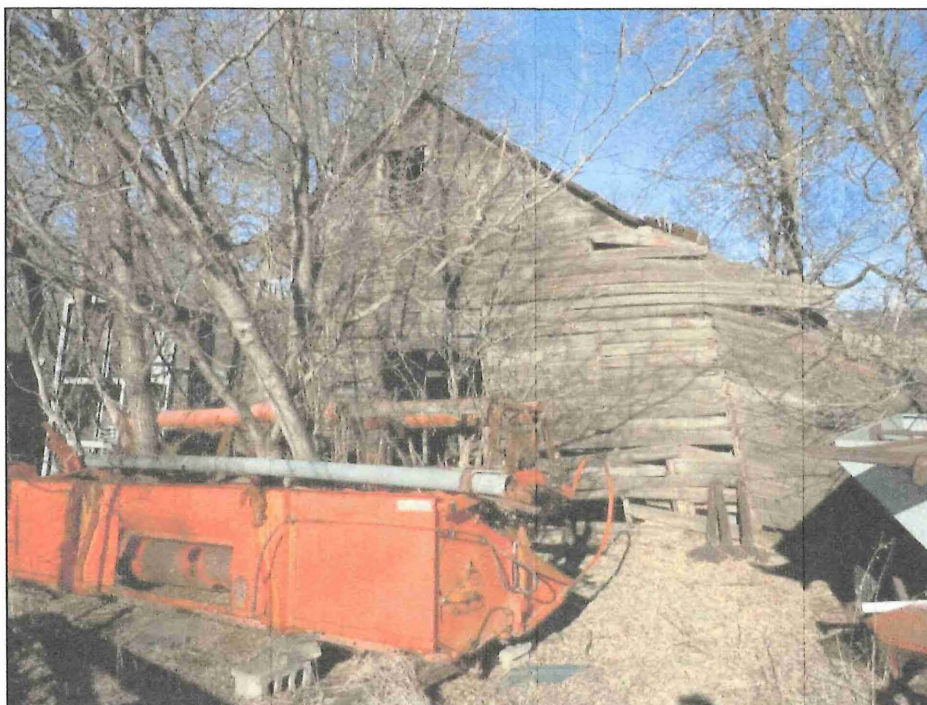


FIGURE 37. 699 45TH STREET NW, POSSIBLE EARLIER GRANARY, LOOKING NE



FIGURE 38. 699 45TH STREET NW, GRANARY, LOOKING SE



FIGURE 39. 699 45TH STREET NW, MACHINE SHED, LOOKING N

door is located on the south end of the east end of the building. Centered on the east end is a bank of three four-light, fixed-sash windows underneath a loft door, suggesting that this building has been repurposed. Two of the same type of window are on the west end of the building. The machine shed has a poured-concrete foundation, and the walls of the building are clad in wood drop siding. The roof is covered in asphalt shingles. A gable-roofed privy superstructure clad in wood drop siding is located next to the northwest corner of the machine shed (Figure 40). It has been moved and is used for storing equipment.

4.5.1 Recommendations

No evidence could be found to suggest that this farmstead was importantly associated with an event or pattern important to the development of Minnesota farming or agriculture in general and therefore it does not meet National Register Criterion A. No information was found to indicate that anyone in the Rasumusson family was historically significant with regard to agriculture, Willmar Township, or otherwise. The farmstead does not clearly illustrate the pattern of features common to a specific type or period of farming; responses to natural, economic, political, or social forces; or any of the other potential means by which a farmstead would possess the distinctive characteristics of a type, period, or method of construction. It does not represent the work of a master, possess high artistic value, or represent a significant and distinguishable entity whose components may lack individual distinction. It therefore does not meet Criterion C. None of the individual elements are especially distinctive examples of architecture or design as required by the Farms Context for significance under Criterion C (Granger and Kelly 2005:7.20). This property is therefore recommended as not eligible for listing in the National Register.

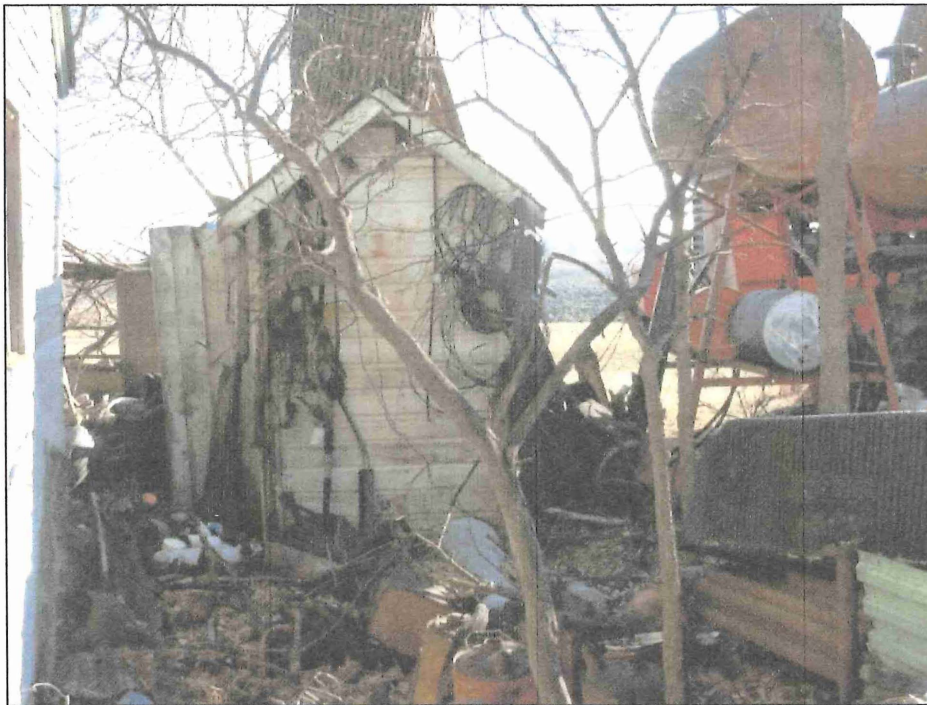


FIGURE 40. 699 45TH STREET NW, PRIVY, LOOKING S

4.6 KH-WLT-004

Farmstead

5840 7th Avenue NW

This property, along with the one to the east at 5812 7th Avenue NW, used to constitute a single farmstead, established sometime between 1886 and 1915. In the latter year, it was owned by Christian J. Hanson, who held the property through at least 1932. In 1954, the property was owned by Johnathan Johnson, who retained it until at least 1961. The current property owner at 5812 7th Avenue NW stated that the original parcel was split the last time it was sold. Buildings located on this parcel include two houses and a barn.

The main house was built circa 1890 (Figure 41). It is one and a half stories in height with a cross-gable roof, and it exhibits a T-plan. What appears to be original wood lap siding is evident, in part, in the north and south gables. Large additions have been made on the north and west sides, the latter a two-bay attached garage, and the front (south) porch has been enclosed. Replacement siding occurs variably as fiberboard, vinyl, and a sheeting that resembles vertical wood planks in the south gable. The roof is covered in asphalt shingles and exhibits vinyl fascia and soffits. All windows and doors are replacement or have been added.

What appears to be a second house, with attached garage, is a side-gabled, converted outbuilding (Figure 42). A sliding machinery door is evident on the east end, where the wall is clad in wood lap siding and a



FIGURE 41. 5840 7th AVENUE NW, HOUSE, LOOKING NW



FIGURE 42. 5840 7TH AVENUE NW, APPARENT SECOND HOUSE, LOOKING NE

concrete-block foundation is visible, as it is on the west. The front (south) of the house has been covered with board and batten siding and a decorative stone treatment between the garage and front door, the latter of which is flanked by added sliding windows. A second pedestrian door is located on the west side of the garage, while a shed-roofed addition is located on the rear of the garage. The roof is clad in asphalt shingles and exhibits vinyl fascia and soffits, which are falling off.

The circa-1945, gothic-arch barn has a poured-concrete foundation (Figure 43). Its lower walls are structural tile, and the walls in the arches are wood siding, the south arch containing drop siding, which is original and deteriorating, and the north arch containing replacement lap siding, which is intact. The roof is covered in wood shakes and features a hay hood over the mow door. Fenestration, where intact, consists largely of original four-light and six-light fixed sash. Some of the windows are no longer present in the south arch, although the openings remain. Dutch doors are located on the east and west sides of the barn, and a pedestrian door is located in the center of the south end. Another pedestrian door opening at the east end of the south end has been infilled with plywood. A sliding door of vertical wood planking is present on the rear of the building.

4.6.1 Recommendations

Extensive alterations have been made to the house and an outbuilding on this property and to the Quonset barn on the neighboring property, and a modern house has been constructed on the neighboring property (see next section). The combination of these occurrences has resulted in a substantial loss of integrity to



FIGURE 43. 5840 7TH AVENUE NW, BARN, LOOKING NE

the farmstead that once comprised both properties, such that it would not be able to convey any potential significance under Criterion A, B, or C. None of the individual elements of the farmstead are especially distinctive examples of architecture or design as required by the Farms Context for significance under Criterion C (Granger and Kelly 2005:7.20). This property is therefore recommended as not eligible for listing in the National Register.

4.7 KH-WLT-005

Farmstead

5812 7th Avenue NW

This property, along with the one to the west at 5840 7th Avenue NW, used to constitute a single farmstead, the history of which is provided in Section 4.6. The current property owner at 5812 7th Avenue NW indicated that the original parcel was split the last time it was sold. Buildings located on this parcel include a modern house (Figure 44) and a Quonset barn.

The circa-1945 Quonset building is clad in corrugated metal and has a concrete-block foundation (Figure 45). Fenestration largely consists of original six-light, fixed sash. Skylights on the west and east sides of the building have been covered with corrugated metal. A wood, apparently original, pedestrian door is located on the rear (north) end of the barn, as is a central overhead door. This door replaces a sliding door, for which the rail support is still present. A sliding door sheathed in corrugated metal is present on the front of the barn, underneath a hinged, vertical wood-plank mow door.



FIGURE 44. 5812 7TH AVENUE NW, MODERN HOUSE, LOOKING NE



FIGURE 45. 5812 7TH AVENUE NW, BARN, LOOKING NE

4.7.1 Recommendations

A modern house has been constructed on this property, which was formerly one with that at 5840 7th Avenue NW. Alterations have been made to the Quonset barn on this property and to the house and an outbuilding on the neighboring property (see previous section). The combination of these occurrences has resulted in a substantial loss of integrity to the farmstead that once comprised both properties, such that it would not be able to convey any potential significance under Criterion A, B, or C. The Quonset barn is not an especially distinctive example of architecture or design as required by the Farms Context for significance under Criterion C (Granger and Kelly 2005:7.20). This property is therefore recommended as not eligible for listing in the National Register.

4.8 KH-WLT-006

Farmstead

5591 7th Avenue NW

Aerial photographs and plat maps indicate that a farmstead was established on this property between 1938 and 1954. Plat maps dating to the latter year and 1957 indicate that it was located on 112 acres owned by George F. Dunlavy, upon which were also located the farmstead described in section 4.10, below.

This former farmstead contains a house, machine shed, and a large modern metal building, which replaces the original barn (Figure 46). The circa-1945, one-and-a-half-story, Minimal Traditional house with attached garage has a concrete-block foundation and is clad in masonite siding (Figure 47). Poured-concrete stairs lead up to an original wood door, sheltered by a metal awning with decorative iron brackets. The cross-gabled roof is covered in asphalt shingles, and a brick interior chimney projects from its south side. Fenestration includes both original and replacement 1/1 double-hung sash windows. The attached single-bay garage has the original wood overhead door and adjacent wood pedestrian door.

The machine shed is a gable-roofed, wood lap-sided building with a poured-concrete foundation (Figure 48). What was once a sliding door of vertical wood planking on the front (north) of the building is now detached from the rail and is propped up by metal poles, as is the pedestrian door on the west side of the building. Fenestration consists of original four-light, fixed sash on the west and east sides of the building. The roof features exposed rafter tails and is covered in asphalt shingles.

4.8.1 Recommendations

The removal of the original barn and its replacement with a large modern metal building leaves only the house and machine shed from the period of significance for Minnesota farms. Although these buildings are generally intact, without other elements of the farmstead they would be unable to convey any potential significance of the farmstead under Criterion A, B, or C, and neither is an especially distinctive example of architecture or design as required by the Farms Context for significance under Criterion C (Granger and Kelly 2005:7.20). This property is therefore recommended as not eligible for listing in the National Register.



FIGURE 46. 5591 7TH AVENUE NW, MODERN METAL BUILDING, LOOKING S



FIGURE 47. 5591 7TH AVENUE NW, HOUSE, LOOKING SE

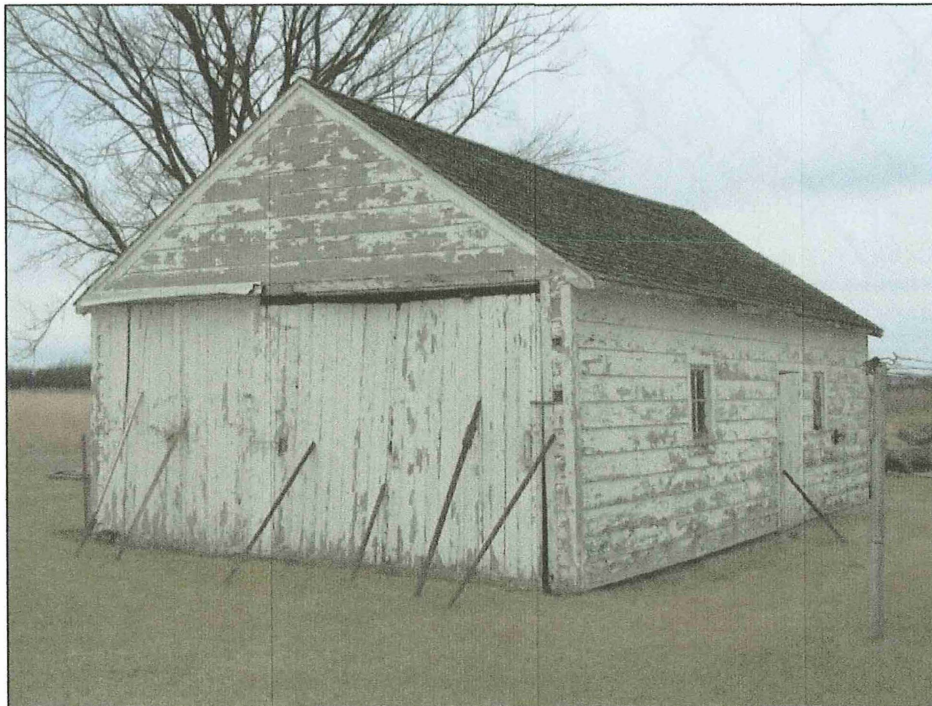


FIGURE 48. 5591 7TH AVENUE NW, MACHINE SHED, LOOKING SE

4.9 KH-WLT-007

Diamond Farm

4611 Highway 12 NW

Access to this property was not permitted by the landowner, requiring the survey to be conducted from publicly accessible points. The property is primarily occupied by a poultry processing facility of the Willmar Poultry Company known as the Diamond Farm, the functions for which are housed in two Quonset buildings, a large rectangular-plan, gable-roofed building, and a small, gable-roofed office building, all of which are located inside a chain-link fence, and all of which have are clad in corrugated metal (Figures 49 to 52). Additionally located on the property, outside of the fence, are a house and a garage.

A 1955 aerial photograph shows the property as having been recently graded and a Quonset building present. By 1963, two much longer Quonset buildings flanked it, one each on the east and west, and the house and garage had been built. Sometime between 1970 and 1991, the two longer Quonset buildings were replaced by one of similar size to the first on the east, and the large gable-roofed building on the west. Because the two existing Quonset buildings are identical, it is likely that the circa-1955 Quonset building was replaced with one of similar size, or at least re-clad during the 1970-1991 period. The one-and-a-half-story, L-plan house and gable-roofed, single-bay garage both appear to have recently received replacement siding, windows, and doors, and asphalt-shingle roof with vinyl soffits and fascia (Figures 53 to 54).



FIGURE 49. DIAMOND FARM, QUONSET BUILDING 1, LOOKING NW

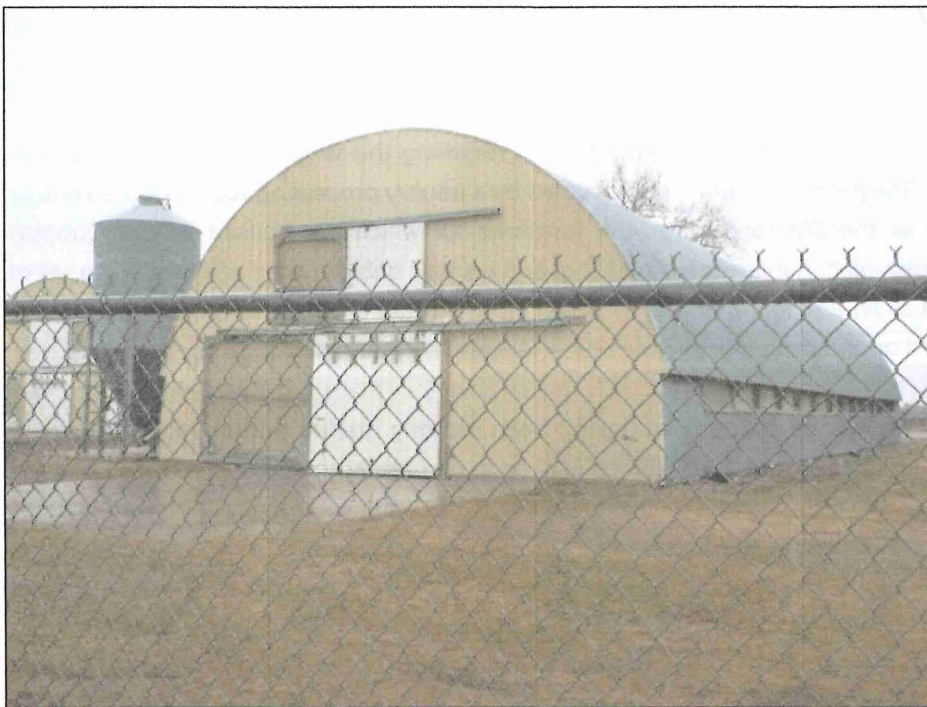


FIGURE 50. DIAMOND FARM, QUONSET BUILDING 2, LOOKING NW



FIGURE 51. DIAMOND FARM, LARGE FACILITY, LOOKING SW



FIGURE 52. DIAMOND FARM, OFFICE, LOOKING W-NW



FIGURE 53. DIAMOND FARM, HOUSE, LOOKING W



FIGURE 54. DIAMOND FARM, GARAGE, LOOKING SE

4.9.1 Recommendations

It is possible that this facility is the outgrowth of an earlier one of the farms established by the Willmar Poultry Company beginning during the late 1950s to handle overages in the poult population (see p. 8). No information, however, could be found to substantiate an association between the owner of the property during the late 1950s and early 1960s, Harold Ditmarson, and the Willmar Poultry Company. Even if such a connection could be made, however, only one of the industrial buildings remaining on this property may date prior to 1970, and if it does, it was likely re-clad after that year. The exterior of the house and the garage have been substantially updated in cladding, fenestration, and roofing. For these reasons, the Diamond Farm would not have the integrity to convey any potential significance under Criterion A, B, or C. This property is therefore recommended as not eligible for listing in the National Register.

4.10 KH-WLT-008

Farmstead

5550 1st Avenue W

A farmstead was established on this property sometime prior to 1886, in which year it was owned by Austin Corbin. By 1915, the property had been transferred to John Anderson, who continued to own the property through at least 1925. In 1932, it was owned by Nels Njus, and in 1954, by Geo. F. Dunlavy, who retained it through at least 1961.

The former farmstead comprises a house, silo, and former granary. The original, circa-1880, T-plan house with cross-gabled roof is two stories in height, but it has been substantially modified through enclosure of the front porch and one-story additions to the east and west ends, the additions on the latter end consisting of an enclosed breezeway and an attached garage (Figure 55). The original construction visible above the additions is clad in wood lap siding, while the newer construction is covered in vinyl siding. Windows are in a multitude of styles and configurations, including a picture window with side casements on the façade; replacement, 1/1 double-hung sash; a bay window on the east end of the east addition; a nine-light fixed-sash window in the breezeway; and a fixed octagonal window in the garage. A newer door is within the front entry. The roof is entirely clad in asphalt shingles, but varying color and condition indicates that portions have been redone more recently than others.

A cement-stave silo is present to the southeast of the house, but the associated barn has been razed (Figure 56). To the southwest of the house is a gable-roofed former granary with shed wing, which is raised on boulders (Figure 57). The gable-roofed portion is clad in wood lap siding on the south and east, and wood drop siding on the north. The shed wing is covered in corrugated metal, as is the roof of the gable-roofed portion. All openings have been infilled with wood planks, except for a 6/6 double-hung sash window on the rear of the building, which appears to have been added, and the pedestrian door on the east side. A dog door has been added to the east side, and this building currently serves as a doghouse. Sliding doors are located on both ends of the shed wing.



FIGURE 55. 5550 1ST AVENUE W, HOUSE, LOOKING N



FIGURE 56. 5550 1ST AVENUE W, SILO, LOOKING E-SE

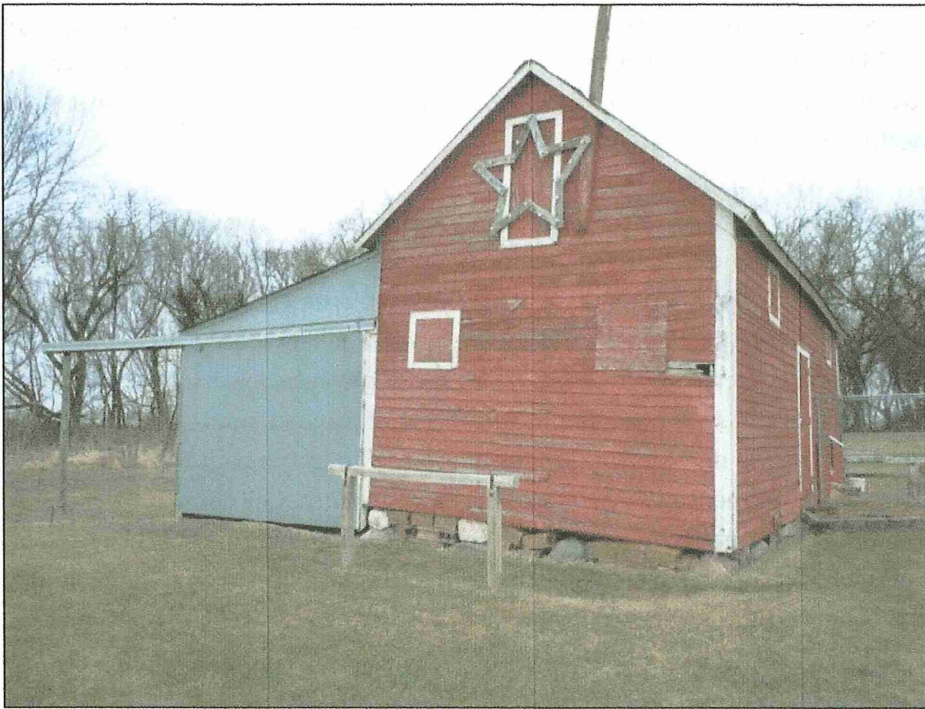


FIGURE 57. 5550 1ST AVENUE W, GRANARY, LOOKING NW

4.10.1 Recommendations

Based on the alterations to the house and granary, combined with the removal of the barn and any other historically present outbuildings, this farmstead would not have the integrity to convey any potential historical significance under National Register Criterion A, B, or C. None of the individual elements of the farmstead are especially distinctive examples of architecture or design as required by the Farms Context for significance under Criterion C (Granger and Kelly 2005:7.20). This property is therefore recommended as not eligible for listing in the National Register.

4.11 KH-WLT-009

Farmstead

4746/4747 1st Avenue W

The ownership of this property on both sides of 1st Avenue West by a single owner began sometime between 1886 and 1915. In the latter year, it was owned by the Anderson Land Company, perhaps affiliated with John Anderson, who owned the property to the west (see Section 4.10). The Anderson Land Company held the property through at least 1925. Between that year and 1932, a house was built on the south side of 1st Avenue West, but in 1932, the property was owned by the State Bank of New London. By 1954, the property, still incorporating both sides of the road, was owned by Harold and Elsie Ditmarson, as was the case through at least 1961.

This former farmstead includes a modern house located on the south side of 1st Avenue W (Figure 58), and a barn, root cellar, and post-1970 machine shed (Figure 59) on the north side of the road. The root cellar, of unknown date, is constructed of poured concrete built into an artificially created hill (Figure 60). A deteriorating plywood pedestrian door provides access to it.

The circa-1930, gothic-arch barn has a concrete-block foundation and lower walls, while the upper walls within the arch are clad in wood drop siding (Figure 61). It incorporates an attached, concrete-block milk house on the north end (Figure 62). Fenestration consists primarily of replacement four-light, fixed-sash windows, two symmetrically placed in each of the north and south arches; regularly spaced along the lower walls on the east and west sides; and one in the north lower wall. One single-light, fixed-sash window has been added to each the lower wall of the south end and the west side of the milk house. A glass-block window is present in the north end of the milk house. The front (south) end of the barn has a pent roof, underneath of which is a central replacement overhead door and a new pedestrian door to its west. A second pedestrian door is located in the east side of the milk house. A larger livestock/machinery door opening on the north end of the barn has been sealed off. The barn roof is clad in asphalt shingles, and a shed-roof dormer that once contained the hay-loft door projects from the west side, the opening now infilled.

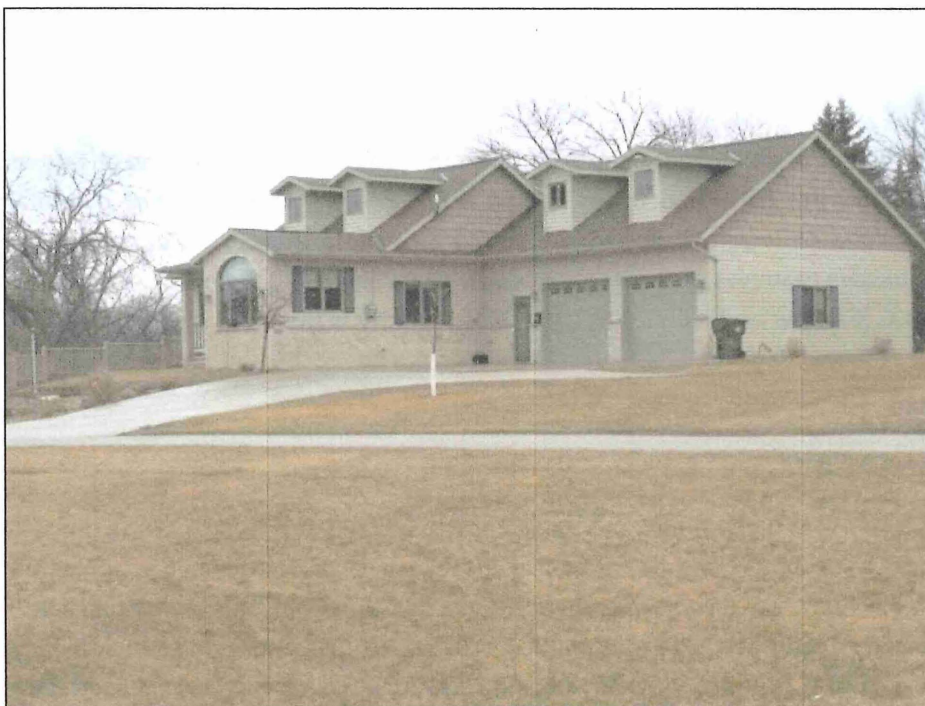


FIGURE 58. 4747 1ST AVENUE W, MODERN HOUSE, LOOKING SW



FIGURE 59. 4746 1ST AVENUE W, MACHINE SHED, LOOKING NW

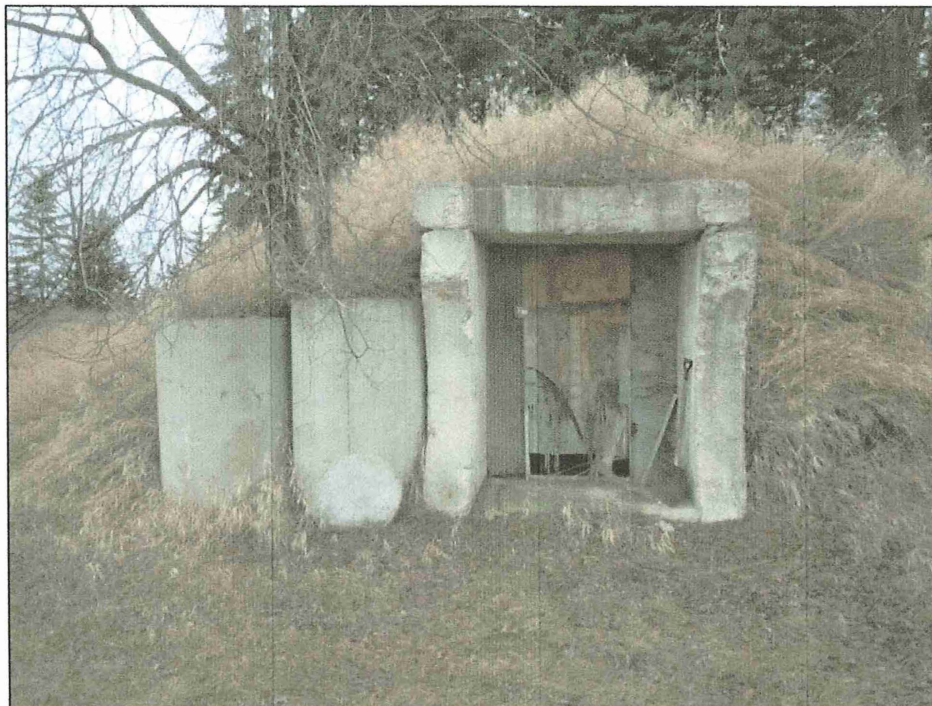


FIGURE 60. 4746 1ST AVENUE W, ROOT CELLAR, LOOKING W

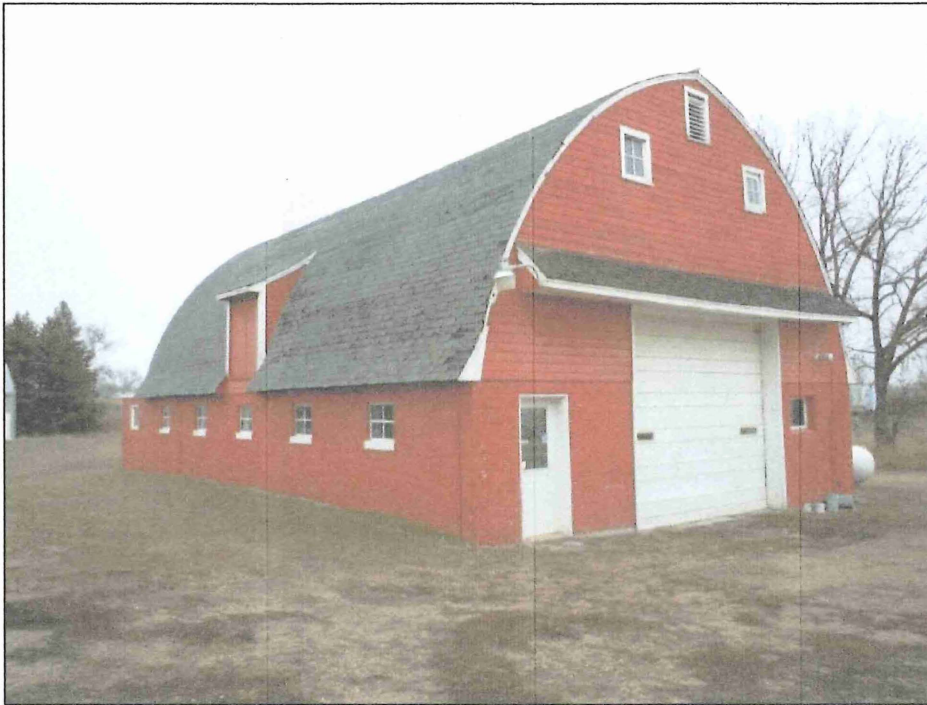


FIGURE 61. 4746 1ST AVENUE W, BARN, LOOKING NE

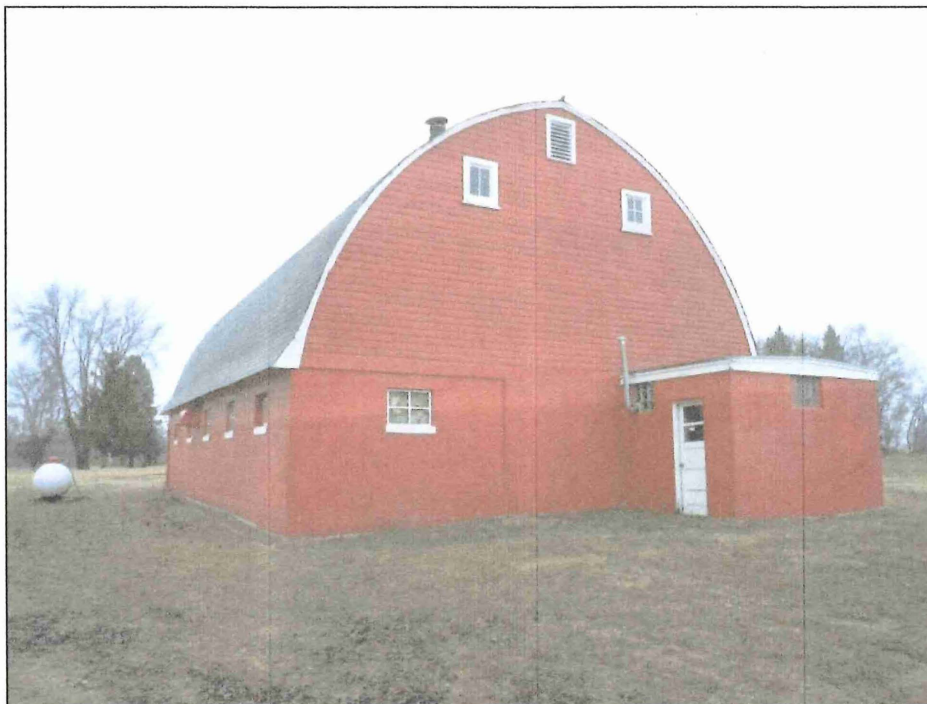


FIGURE 62. 4746 1ST AVENUE W, BARN AND MILK HOUSE, LOOKING SW

4.11.1 Recommendations

Only the barn, which has been altered, and possibly the root cellar remain standing from the period of significance for Minnesota farms; without any other elements of the farmstead present, they would be unable to convey any potential significance of the farmstead under Criterion A, B, or C. Neither is an especially distinctive example of architecture or design as required by the Farms Context for significance under Criterion C (Granger and Kelly 2005:7.20). This property is therefore recommended as not eligible for listing in the National Register.

4.12 KH-WLT-010

Farmstead

1399 45th Street SW

A farmstead was established in this location sometime prior to 1886, as indicated by the house pictured on the plat map of that year, but the 40 acres within which the house was located, along with the adjacent 120 acres to its northeast, were by that time owned by the St. Paul Minneapolis and Manitoba Railway Company. Between 1886 and 1915, the parcels were reconfigured, and in the latter year, the house and its 40 acres, along with the adjacent 40 acres on the north and east, were owned by Minnie Weber et al., which continued to be the case through at least 1925. In 1932, the 120 acres were owned by the Universalist General Convention, a religious organization, although map symbology indicates that at least the house was owned by the property owner who held the 160 acres to the south, Berthal K. Arvig. As of 1954, the 120 acres and house were owned by Herman and Lydia Pehling, who remained the property owners through at least 1961.

This farmstead property includes a house, garage, two chicken coops, and seven outbuildings. Aerial photographs indicate that the garage, chicken coops, and one of the outbuildings, a modern metal building, were all constructed after 1991, during which period a barn with silo in the vicinity of the metal building was torn down (Figures 63 to 65).

The cross-gabled house is a variable one and a half stories in height, as the gable on the west section of the house does not extend to the height of the others (Figure 66). The upper levels of the higher-gabled sections are incongruous, but original wood window trim around the windows under and near the north-side gable suggests this exterior wall is of the original configuration; the remainder of the upper north wall and the upper walls on the south and east sides either lack fenestration or have newer windows and trim, which along with the incongruity suggests that the upper levels were built up and out from the original configuration on these sides (Figures 67 and 68). The windows on the lower level also have been replaced, with the exception of a stained-glass window located on the east side of the house, which remains above a newer picture window surrounded by the original wood trim. Similarly, the replacement door to its north is within the original wood door frame. This side of the house features an open porch, the roof supported by spindlework posts and situated under a newer balcony. A deck has been built to extend off of the porch. An open porch with similar posts and also extended by a deck is located on the south side of the west



FIGURE 63. 1399 45TH STREET SW, MODERN GARAGE, LOOKING SE



FIGURE 64. 1399 45TH STREET SW, CHICKEN COOPS, LOOKING NE

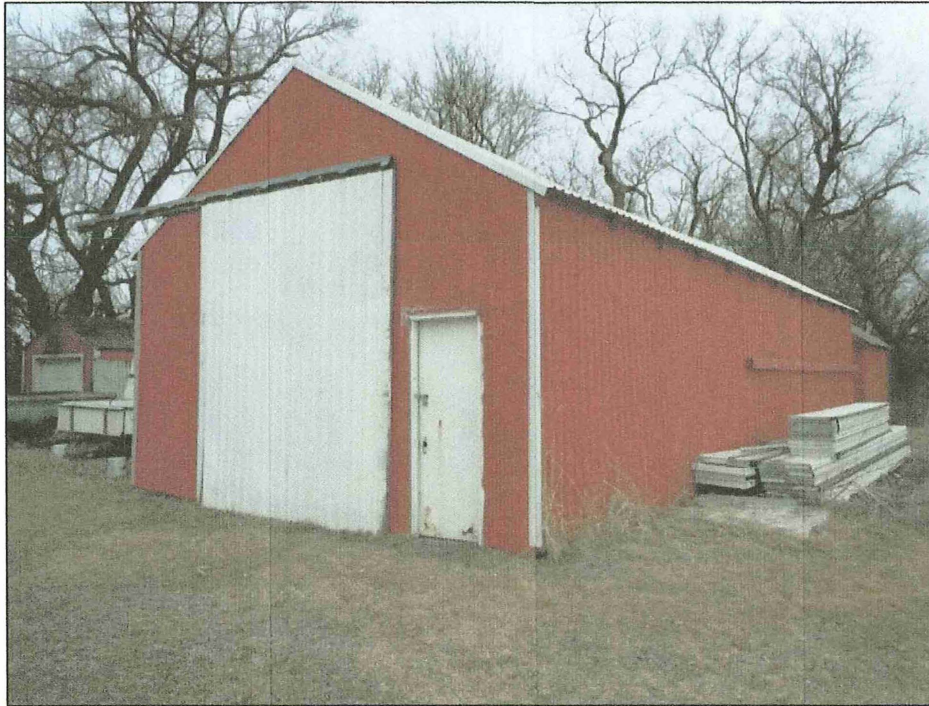


FIGURE 65. 1399 45TH STREET SW, MODERN METAL OUTBUILDING, LOOKING NW



FIGURE 66. 1399 45TH STREET SW, HOUSE, LOOKING NE



FIGURE 67. 1399 45TH STREET SW, HOUSE, LOOKING S



FIGURE 68. 1399 45TH STREET SW, HOUSE, LOOKING W-NW

section, and the wall here contains a replacement window and door. Replacement windows are located on the remaining walls of this section, although those on the north side are surrounded by original wood trim. The walls of the house are wood lap siding and are supported on a stone foundation. The roof is covered in asphalt shingles, and an interior brick chimney projects from its peak above the west section.

Approximately 150 feet northeast of the house is a front-gabled outbuilding on a concrete foundation (Figure 69). It is clad in a variety of materials due to any number of alterations. Wood drop siding is present on the west wall, which also features sliding, vertical wood-plank doors. The north wall and the roof are covered in corrugated metal, while the south side features board-and-batten siding. This side contains the only window opening, which is framed for a fixed four-light window, but the glass has broken out and the opening covered over from the interior.

To the west of the house is a semicircle of four outbuildings, all of which have had overhead doors added to them and now resemble garages. From south to north, the first of these outbuildings is a front-gabled, two-bay garage clad in board-and-batten siding (Figure 70). It has no foundation but is raised on wood blocks. The roof is covered in asphalt shingles and has plywood soffits. Two single-light, fixed windows on the south wall constitute the only fenestration on this building. The next, front-gabled outbuilding is one and a half stories high, features a poured-concrete foundation, and appears to be a former poultry house (Figure 71). Fenestration consists of six six-light, fixed-sash windows on the south side and one of the same type on the west end. On the east end, a loft door is situated over the added metal overhead door. A vertical wood-plank pedestrian door is present on the north side. This building is clad in wood lap siding and has an asphalt-shingle roof. The third in this group of outbuildings is also front gabled (Figure 72). It is raised on boulders. Original wood lap siding is evident on the north end; the remainder has been clad in vinyl siding. The roof is covered in corrugated metal. This building has no windows, but a wood, single-panel pedestrian door containing a six-light window is located on the south side. The final building in this group features a substantial poured-concrete foundation, which appears would have supported a superstructure more substantial than the front-gabled, half-story one clad in wood drop siding which is currently present (Figure 73). Two windows are present on the building, a four-light, fixed-sash window on the south end, and a single-light replacement window on the west side. The edge of a pedestrian door is visible on the east side, but its details were obscured by a tall fence. The roof features exposed rafter ends and is clad in asphalt shingles.

Nearly adjacent to and north of the modern metal building is a granary, which aerial photographs indicate was moved to its current location after 1991 (Figure 74). The circa-1920, gable-roofed granary is raised on wood skids and clad in wood drop siding. A single window opening is present in the south gable, but the window is no longer present. A pedestrian door has been fashioned by making cuts in the siding on the east side. The roof is clad in asphalt shingles and features exposed rafter tails.

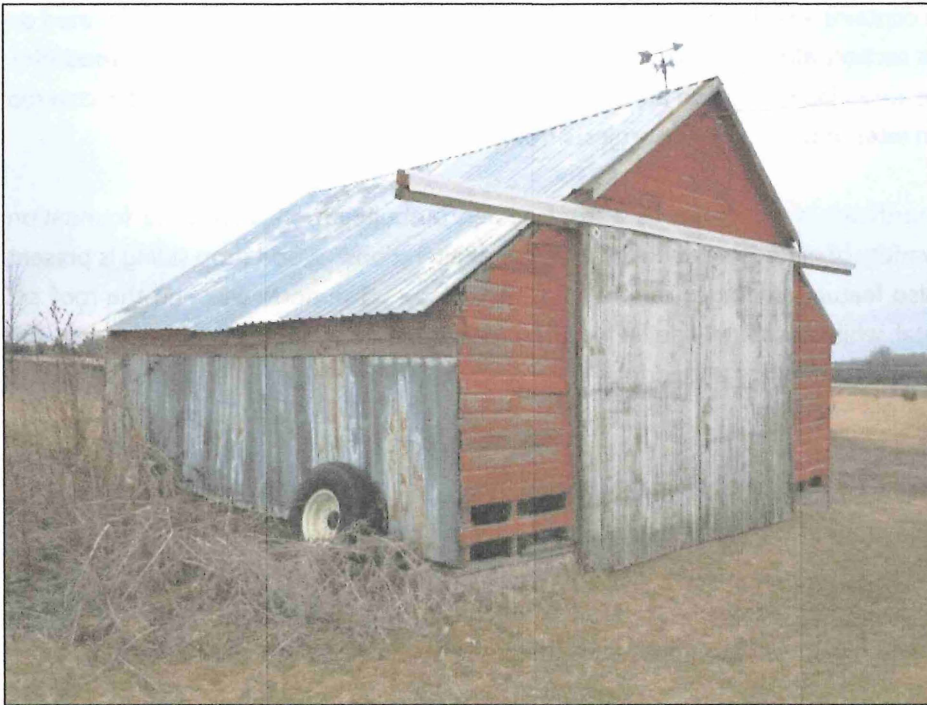


FIGURE 69. 1399 45TH STREET SW, OUTBUILDING, LOOKING SE



FIGURE 70. 1399 45TH STREET SW, GARAGE-LIKE BUILDING 1, LOOKING SW



FIGURE 71. 1399 45TH STREET SW, GARAGE-LIKE BUILDING 2, LOOKING NW

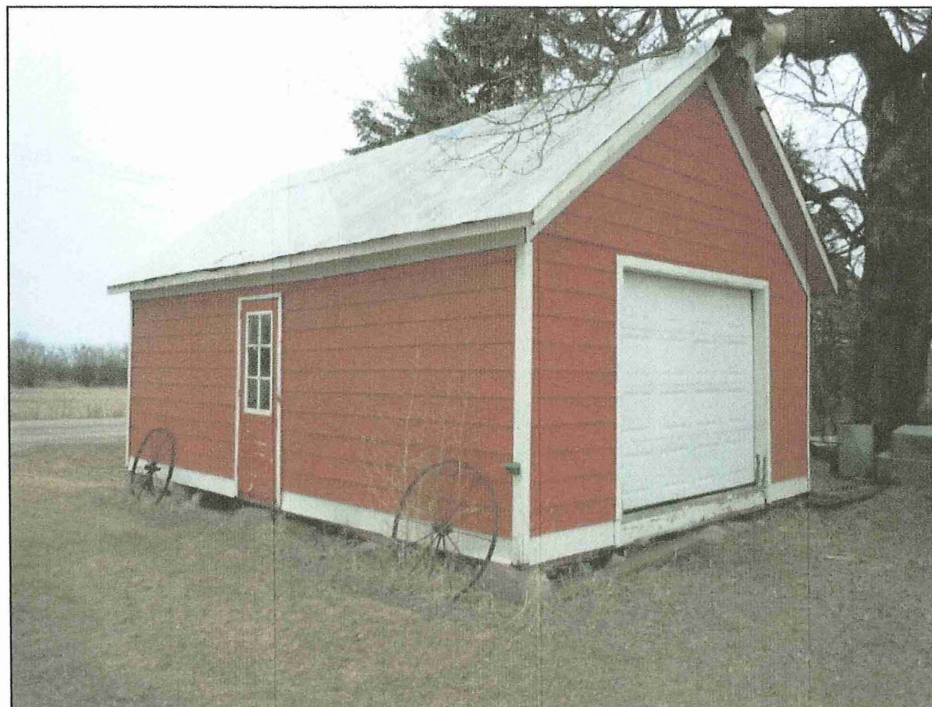


FIGURE 72. 1399 45TH STREET SW, GARAGE-LIKE BUILDING 3, LOOKING NW

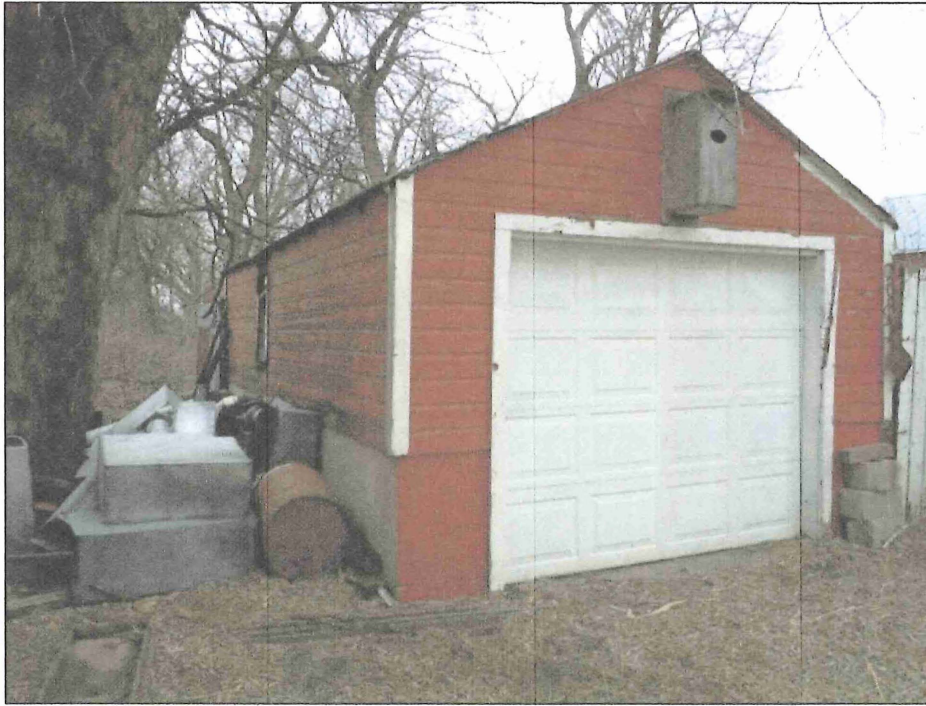


FIGURE 73. 1399 45TH STREET SW, GARAGE-LIKE BUILDING 4, LOOKING NE

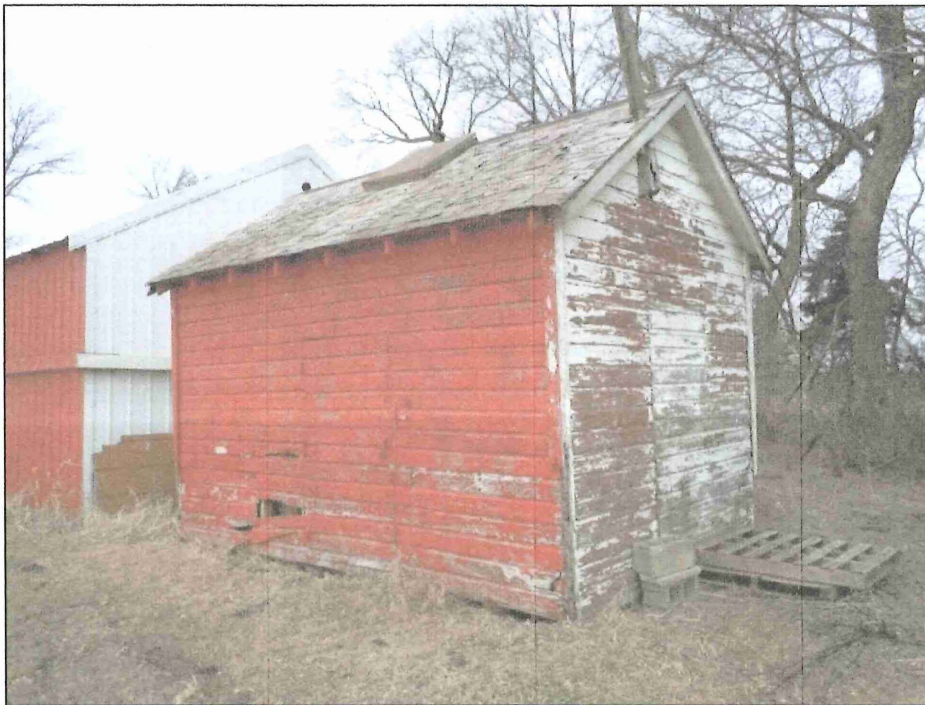


FIGURE 74. 1399 45TH STREET SW, GRANARY, LOOKING SW

4.12.1 Recommendations

Extensive alterations have been made to the house and the majority of the outbuildings on this farmstead. Due to these alterations, the loss of the barn and silo, and the addition of the modern metal building and garage, this farmstead would not have the integrity to convey any potential significance under Criterion A, B, or C. None of the individual elements of the farmstead are especially distinctive examples of architecture or design as required by the Farms Context for significance under Criterion C (Granger and Kelly 2005:7.20). This property is therefore recommended as not eligible for listing in the National Register.

4.13 KH-WLT-011

Farmstead

1560 45th Street SW

Historical plat maps and aerial photographs indicate that a farmstead was established in this location between 1932 and 1938. The property owner as of 1954 was Thomas Erickson, and he remained so through at least 1961.

The former farmstead is now a tree service yard, but a house, barn, and poultry house are present on the property, along with a modern metal building that serves as the office for the tree service company (Figure 75). Whatever the original appearance of the house, it is now obscured by vinyl siding, new windows and doors, a new asphalt shingle-covered roof with vinyl soffits, and expansion horizontally and perhaps vertically as well (Figure 76).

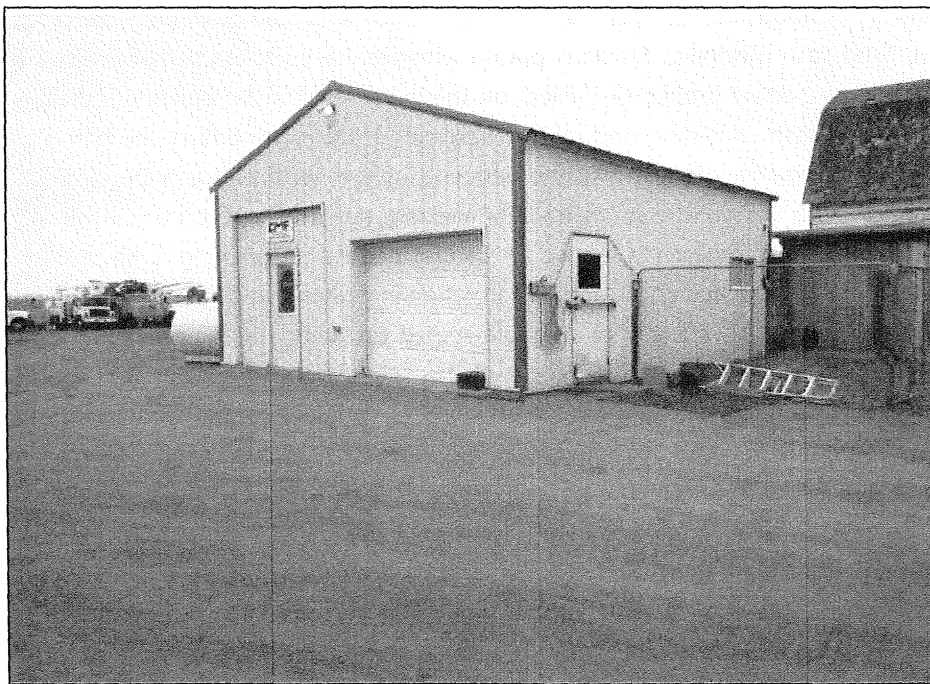


FIGURE 75. 1560 45TH STREET SW, MODERN METAL BUILDING, LOOKING SW



FIGURE 76. 1560 45TH STREET SW, HOUSE, LOOKING NE

The barn has not been maintained and is in a state of deterioration (Figure 77). It features a poured-concrete foundation and wood drop siding. The gambrel roof is clad in wood shakes and exhibits exposed rafter tails. On the north end, a hay hood extends over the mow door, which is flanked by window openings containing broken six-light, fixed-sash windows. Similarly placed windows have broken out of the south end. What remains of the windows, either broken or infilled, on the lower level of the building indicates that these were also six-light fixed sash. A sliding door at the west end of the north end of the barn has come off the rails and is propped against the wall. At the east end of the north end is a Dutch door. Pedestrian door openings are at both ends of the south wall, the western one of which contains a vertical wood-plank door, and the eastern one of which is empty. A central pedestrian opening is empty and blocked by debris. The remnant of an attached gable and discoloration indicates that this central opening provided a connection between the main barn and a small, gable-roofed extension on its south end.

Aerial photographs demonstrate that the poultry house, although likely original to the farmstead, was moved to its current location sometime between 1970 and 1991 (Figure 78). The gabled roof is covered in asphalt shingles and features exposed rafter tails. The building has a poured-concrete foundation and is clad in wood drop siding. Regularly spaced, original six-light, fixed-sash windows are present on the east side of the building, and a single four-light, fixed-sash window is located on the south end. No window openings are present on the north or west side. A sliding door, sheathed in corrugated metal, is located on the north end of the building and a narrow pedestrian door of vertical wood planking is on the east side.



FIGURE 77. 1560 45TH STREET SW, BARN, LOOKING SW



FIGURE 78. 1560 45TH STREET SW, POULTRY HOUSE, LOOKING W

The south end features a Dutch door, also of vertical wood planking. Loft doors are present in the north and south gables.

4.13.1 Recommendations

Extensive alterations have been made to the house on this property, and the poultry house was moved after the period of significance for Minnesota farms. These occurrences, combined with the removal of other outbuildings and the conversion of the property to a tree service yard have resulted in a substantial loss of integrity to the farmstead, such that it would not be able to convey any potential significance under Criterion A, B, or C. Although the barn and poultry house are generally intact, neither constitutes an especially distinctive example of architecture or design as required by the Farms Context for significance under Criterion C (Granger and Kelly 2005:7.20). This property is therefore recommended as not eligible for listing in the National Register.

4.14 KH-WLT-012

Farmstead

2280 45th Street SW

Historical plat maps and aerial photographs indicate that this farmstead was established between 1925 and 1932, and in the latter year, it was owned by Rose Herman. By 1954, ownership had transferred to J. W. and Seina Boog, who retained the property until at least 1961.

The farmstead includes a complement of buildings, about half of which are modern, and several corrugated-metal grain bins (Figure 79). The modern buildings, some of which date to the twenty-first century, are all large metal buildings (Figures 80 to 84). The remaining buildings are the house, a likely milk house, a granary, a small barn, and a Quonset outbuilding. Although the current property owner indicated a construction date of circa 1945 for the one-and-a-half-story, gable-roofed house, a circa-1930 date is supported by a 1932 plat map and 1938 aerial photograph. In either case, it has been substantially altered through the addition of an enclosed front porch, the construction of one-story additions on the south side and rear (west) end, the cladding of its entirety in vinyl siding, the installation of replacement windows and doors, and the installation of vinyl soffits and fascia (Figure 85). The foundation of the house is concrete block, and the roof is covered with asphalt shingles. Shed dormers project from each side of the roof, and a brick interior chimney rises above the peak.

An outbuilding that the current owner's children used as a playhouse, perhaps originally a milk house, is located near the eastern end of the farmstead (Figure 86). It has a low-pitched, gabled roof, wood lap siding, and a single pedestrian door of vertical wood planks. The roof is covered in asphalt shingles.

Beyond the modern metal buildings to the west of the possible milk house is a granary, which similarly has a gabled roof, wood lap siding, and a roof covered in asphalt shingles (Figure 87). The roof, however, has a higher pitch and features exposed rafter tails. A sliding door of vertical wood planks is located on the south side of the building, and the superstructure rests on concrete skids.



FIGURE 79. 2280 45TH STREET SW, GRAIN BINS, LOOKING NW

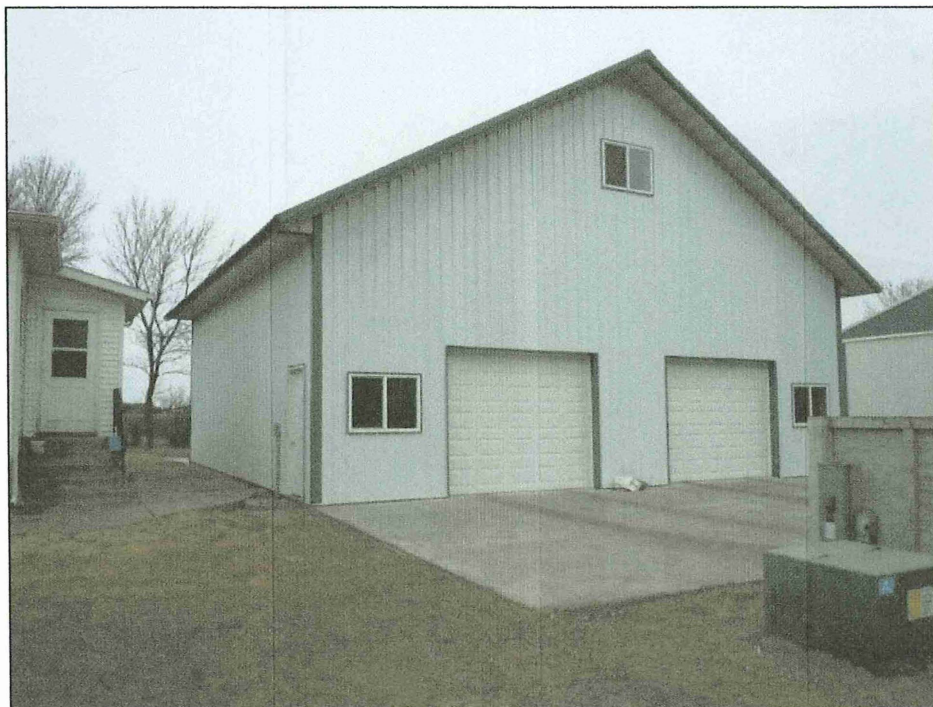


FIGURE 80. 2280 45TH STREET SW, MODERN METAL BUILDING 1, LOOKING SW

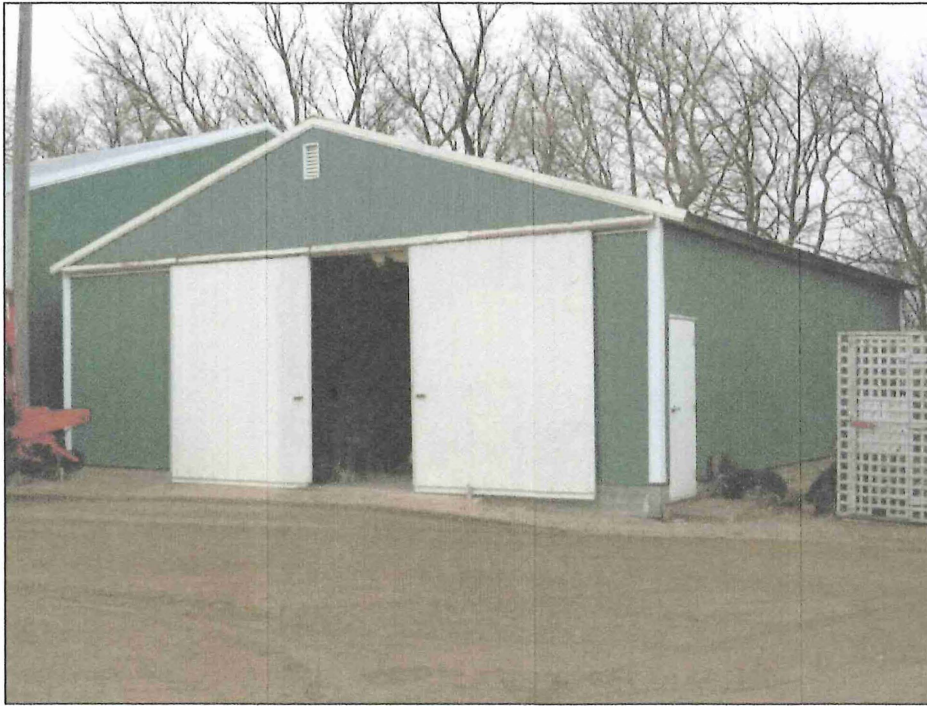


FIGURE 81. 2280 45TH STREET SW, MODERN METAL BUILDING 2, LOOKING NW



FIGURE 82. 2280 45TH STREET SW, MODERN METAL BUILDING 3, LOOKING SW



FIGURE 83. 2280 45TH STREET SW, MODERN METAL BUILDING 4, LOOKING NE



FIGURE 84. 2280 45TH STREET SW, MODERN METAL BUILDING 5, LOOKING SE



FIGURE 85. 2280 45TH STREET SW, HOUSE, LOOKING NW



FIGURE 86. 2280 45TH STREET SW, "PLAYHOUSE," LOOKING NE



FIGURE 87. 2280 45TH STREET SW, GRANARY, LOOKING NE

A bit farther west is the small barn, which has a poured-concrete foundation (Figure 88). Its gable roof is clad in asphalt shingles and has exposed rafter tails, and the walls are clad in wood drop siding. Original four-light, fixed-sash windows are present on the sides and in the south gable, though some of the panes have been broken out. The window in the gable is centered over a loft door, which in turn is over a Dutch door, both doors made of vertical wood planks. Crude openings have been cut into the base of the superstructure on the east side, at least one of which is boarded in.

The Quonset building, which dates to circa 1960, has a poured-concrete foundation and corrugated-metal siding (Figure 89). Sliding doors are located on the south end, above which are two original four-light, fixed-sash windows.

4.14.1 Recommendations

Extensive alterations have been made to the house, and several massive, modern outbuildings have been constructed on and dominate the visual aspect of this property. The farmstead would therefore be unable to convey any potential significance under Criterion A, B, or C. None of the individual elements of the farmstead are especially distinctive examples of architecture or design as required by the Farms Context for significance under Criterion C (Granger and Kelly 2005:7.20). This property is therefore recommended as not eligible for listing in the National Register.



FIGURE 88. 2280 45TH STREET SW, SMALL BARN, LOOKING NE

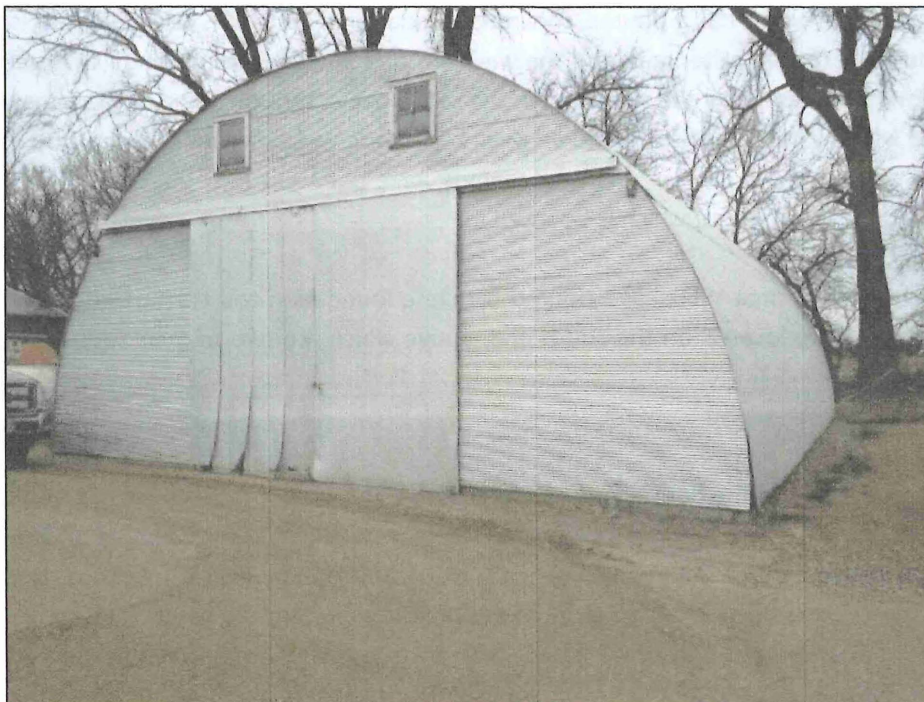


FIGURE 89. 2280 45TH STREET SW, QUONSET, LOOKING NW

4.15 KH-WLT-013

House

2460 45th Street SW

This one-story, Ranch-style house with concrete-block foundation and attached two-bay garage was built circa 1965 (Figure 90). The walls are clad in masonite siding and the side-gabled roof in asphalt shingles; the roofing is newer, and the soffits and fascia are vinyl. A brick interior chimney extends above the roof. At the front of the house, poured-concrete steps lead up to a centrally placed door. Fenestration consists primarily of original casement windows, quadrupled to create a picture window on the façade and paired otherwise. Single-pane, fixed-sash windows are located in the garage and at the basement level on the north and west sides of the house. The garage has two replacement overhead doors, next to which on the north is a pedestrian door.

4.15.1 Recommendations

This house has no important associations with historically significant events, patterns, or persons, and it therefore does not meet National Register Criterion A or B. It is a modest, fairly non-descript house that alludes to but does not exhibit any notable architectural hallmarks of the Ranch style; therefore it does not meet Criterion C. This property is therefore recommended as not eligible for listing in the National Register.



FIGURE 90. 2460 45TH STREET SW, HOUSE, LOOKING SW

4.16 KH-WLT-014

Barn

45th Street SW, north of BNSF line

A farmstead was established in this location between 1886 and 1915 and was owned by Andrew Larson in the latter year through at least 1925, according to historical plat maps. In 1932, the property was owned by Clara Tallman, and then in 1954 by Horace and Alice Hartjen. By 1957, ownership had transferred to E. T. Krumweide.

Access to this property was not permitted by the landowner, requiring the survey to be conducted from 45th Street SW. This former farmstead has been reduced by the removal of all other buildings to a single, abandoned barn (Figure 91), which aerial photographs indicate was built in this location sometime between 1938 and 1955. The barn, gable-roofed with a shed wing, has a poured-concrete foundation (Figure 92). The walls are clad in wood lap siding. Window openings have all been infilled, except in two cases on the south side, in which the infill has been broken out. A pedestrian doorway on the east side of the barn also has been infilled. A partially intact Dutch door and a pedestrian door are present on the north side of the building, while a sliding door is located on the south. Two loft doors are located on each end of the barn in positions that mirror each other, near the junction of the main massing and the shed wing, although one on the north end is broken out. The roof of the barn is clad in wood shakes, and it features exposed rafter tails and a hay hood over the mow doors on the south.



FIGURE 91. BARN, 45TH STREET SW, LOOKING NW

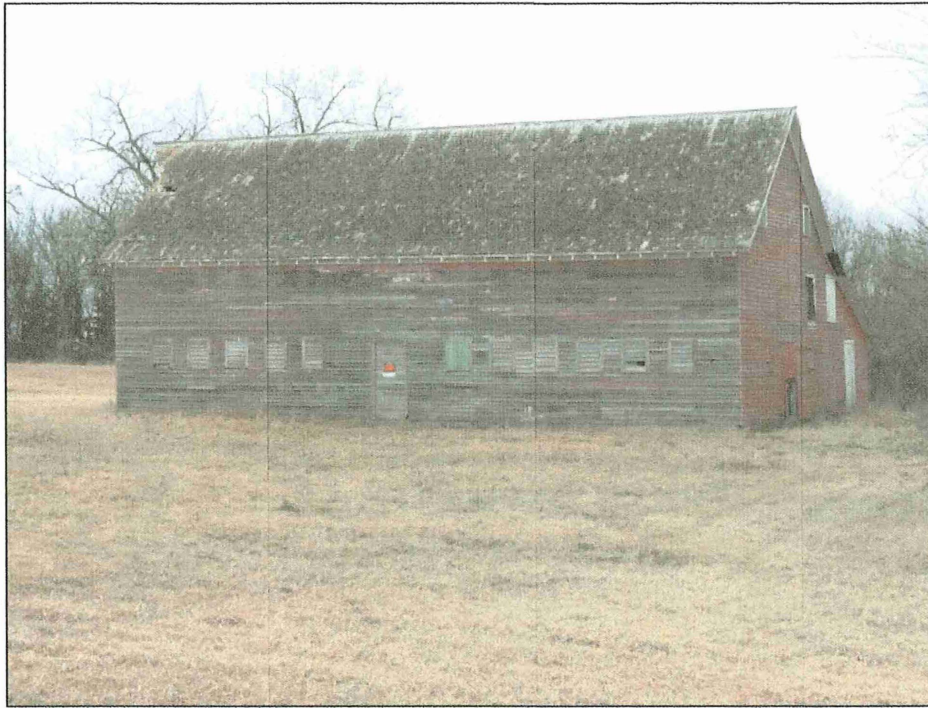


FIGURE 92. BARN 45TH STREET SW, LOOKING SW

4.16.1 Recommendations

With only a barn present, this property would not have the integrity to convey any potential significance of the former farmstead under Criterion A, B, or C. The barn is not an especially distinctive example of architecture or design as required by the Farms Context for significance under Criterion C (Granger and Kelly 2005:7.20). This property is therefore recommended as not eligible for listing in the National Register.

4.17 KH-WLT-015

Farmstead

3830 County Road 15 SW

Although a house undoubtedly associated with a farmstead was present on the 80 acres within which this property is located in 1886, maps dating to 1915 and 1925 do not show a house here. The property owner in 1915 and 1925 was Harold B. Handy, as was the case in 1932, the next year in which a plat map shows a house at this location. In 1954, the farmstead was owned by Gilbert and Bertha Marzuardt, and then in 1957 by E. and E. Levik.

This former farmstead comprises a house, garage, a machine shed, a loafing shed, and two outbuildings. The one-and-a-half-story house with attached garage, if not constructed in the modern era, has been substantially expanded and updated such that any original construction is completely obscured (Figure 93).



FIGURE 93. 3830 COUNTY ROAD 15 SW, HOUSE, LOOKING E

It has a side-gabled roof with four front-facing and one rear-facing gabled dormers, an open front porch in front of a recessed front entry, and a small perpendicular wing off the back (northeast) side of the house, at the end of which is a wide, brick exterior chimney. It is clad in vinyl siding, has newer windows and doors throughout, and has vinyl soffits and fascia. The foundation is concrete block.

A gable-roofed building with overhead door that appears as a detached garage is located to the east of the house; however, no driveway leads to it, suggesting that it currently serves some other function (Figure 94). The walls are wood lap siding, except in the gable, which may constitute an addition to the original building. The gable contains replacement siding underlined by wood shakes, which angle down and out from the base of the gable to the edges of the eaves, which have vinyl soffits. Within the gable is a nine-light casement window. The gabled roof has vinyl fascia and soffits, and is clad in asphalt shingles.

Across the driveway from the house to the southwest is a large, gable-roofed outbuilding clad in metal siding (Figure 95). It features a large central overhead door, flanked on either side by newer pedestrian doors and beyond those, new windows. Historical aerial photographs show a large barn in this location, but it is oriented on the opposite axis. This building therefore either represents a post-1963 reconfiguration or post-1963 construction.



FIGURE 94. 3830 COUNTY ROAD 15 SW, GARAGE-LIKE BUILDING, LOOKING SE



FIGURE 95. 3830 COUNTY ROAD 15 SW, METAL OUTBUILDING, LOOKING SW

The remaining outbuildings and structures are to the northwest: a circa-1945, side-arched Quonset machine shed clad in corrugated metal with a poured-concrete foundation (Figure 96); a shed-roofed loafing shed clad in corrugated metal (Figure 97); and a side-gabled outbuilding with wood drop siding and a poured-concrete foundation (Figure 98). The latter may contain original four-light windows in the southeast end, but several other windows and the loft door have been infilled with plywood. Two doors are located on the southwest side of the building, a pedestrian door of vertical wood planking and a wider hinged door of the same. A small, informal shade has been made by extending a corrugated-metal sheet in a shed-roofed fashion from the northwest end of the southwest face of the building to connect to wooden fencing a few feet to its southwest.

4.17.1 Recommendations

Based on the extensive alterations to or replacement of the house and the main barn, alterations to the side-gabled outbuilding, and the loss of other outbuildings that are evident in historical aerial photographs, this property would not have the integrity to convey any potential significance of the former farmstead under Criterion A, B, or C. None of the individual elements of the farmstead are especially distinctive examples of architecture or design as required by the Farms Context for significance under Criterion C (Granger and Kelly 2005:7.20). This property is therefore recommended as not eligible for listing in the National Register.



FIGURE 96. 3830 COUNTY ROAD 15 SW, MACHINE SHED, LOOKING N

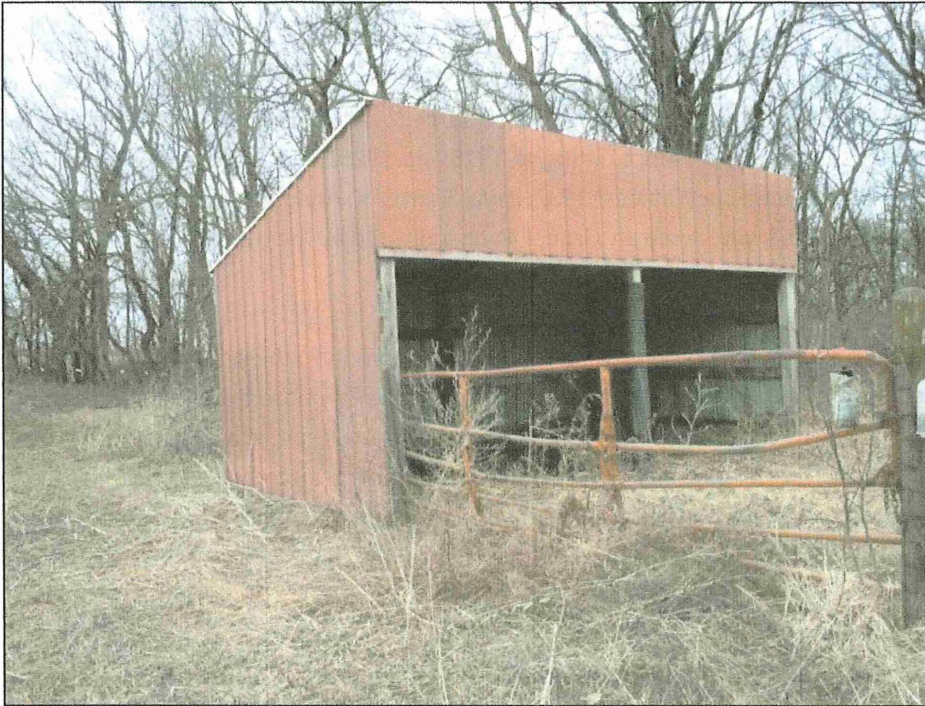


FIGURE 97. 3830 COUNTY ROAD 15 SW, LOAFING SHED, LOOKING SE



FIGURE 98. 3830 COUNTY ROAD 15 SW, OUTBUILDING, LOOKING E

4.18 KH-WLT-016

Farmstead

3843 15th Avenue SW

The 1886 plat map depicting this property shows a house in the same location as on plat maps dating to 1932 and later, although no house is shown on maps dating to 1915 and 1925. The property owner in 1886 was Christen Arneson. In the year 1915 through at least 1932, it was Berthal K. Arvig. The property remained in the Arvig family through at least 1961, with Jesse and Isabel Arvig indicated as the owners on maps dating between 1954 and 1961.

Access restrictions were placed on the survey of this former farmstead, preventing the obtainment of details on each of the buildings and structures. Comparison, however, of recent aerial photography shows that the house currently present is of very recent construction (Figures 99 and 100), as is a greenhouse to its southwest (Figure 101), all but one outbuilding has been removed, and nearly all trees have been cleared. The only remnants of the farmstead are a partial cement-stave silo (Figure 102) and a corrugated-metal machine shed (Figure 103).

4.18.1 Recommendations

Based on the extensive alterations to this property, it would not have the integrity to convey any potential significance of the former farmstead under Criterion A, B, or C. Neither the silo nor the machine shed is an especially distinctive example of architecture or design as required by the Farms Context for significance under Criterion C (Granger and Kelly 2005:7.20). This property is therefore recommended as not eligible for listing in the National Register.



FIGURE 99. 3843 15TH AVENUE SW, RECENT BIRDS' EYE VIEW (LEFT) AND 2016 AERIAL (RIGHT) (NORTH IS TO THE RIGHT)



FIGURE 100. 3843 15TH AVENUE SW, HOUSE, LOOKING SW

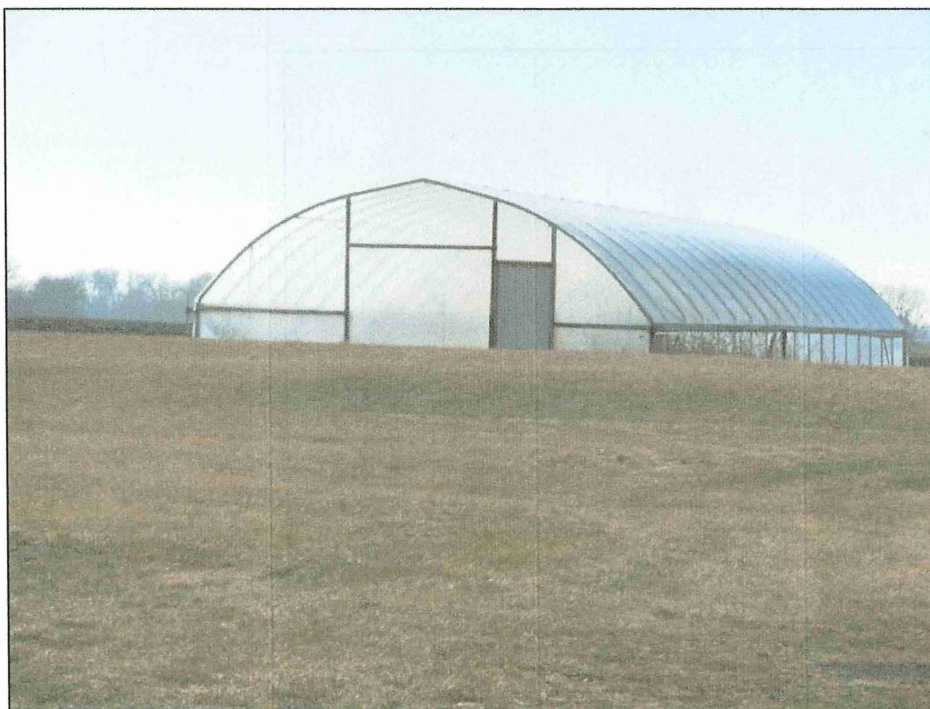


FIGURE 101. 3843 15TH AVENUE SW, GREENHOUSE, LOOKING SW



FIGURE 102. 3843 15TH AVENUE SW, SILO, LOOKING SE



FIGURE 103. 3843 15TH AVENUE SW, MACHINE SHED, LOOKING SW

4.19 KH-WLT-017

3631/3637 15th Avenue SW

No buildings are shown at this location on plat maps dating through 1932. A house and single outbuilding can be seen on the 1938 aerial photograph of the property, and what appears to be a farmstead is visible in this location on a 1955 aerial photograph. In 1954, the earliest plat map to show a house at this location, it was within a five-acre parcel owned by Theda Klosterboer, with the remainder of the quarter-section owned by Harvey and Della Wibbels, which was also the case in 1961.

A response regarding access to this property was not provided by the landowner. For this reason, and for safety reasons given the condition of and materials on this property, the survey was conducted from the driveway. This former farmstead currently includes two houses, a garage, and an outbuilding, and it currently functions as a junkyard. The house farthest from the road is nearer to the outbuilding, and was likely built circa 1938, as only the house and outbuilding appear on the aerial photograph of that year (Figure 104). This side-gabled, one-and-a-half-story house, which has a concrete-block foundation, has been covered with vinyl siding and has replacement windows and doors. The roof, which features a shed dormer on each side, is clad in asphalt shingles and has vinyl fascia and soffits.

The front-gabled outbuilding is constructed of concrete block, and the gable is clad in replacement siding, likely metal (Figure 105). The gable contains metal-sheathed loft doors, which are the only indicator that this building may once have served an agricultural function. It now appears to be a storage/office building, with a newer garage door on the east end and a newer pedestrian door on the north side. The window openings on this north side have been infilled. An addition off the west end of the building was put on sometime between 1970 and 1991.

A second one-and-a-half-story house, this one cross-gabled, is located nearer to the road, and aerial photographs show it was put in place circa 1960 (Figure 106). Based, however, on its appearance, which does not reflect architecture of that era, it was moved from another location. This house similarly is on a concrete-block foundation, has been clad in vinyl siding, and has replacement doors and windows; an asphalt-shingle roof, and vinyl fascia and soffits. The detached two-bay garage off of its southwest corner was built sometime after 1991 (Figure 107).

4.19.1 Recommendations

Given the extensive alterations to this property, both to the buildings and as a whole, it would not have the integrity to convey any potential significance of the former farmstead under Criterion A, B, or C. None of the individual elements are especially distinctive examples of architecture or design as required by the Farms Context for significance under Criterion C (Granger and Kelly 2005:7.20). This property is therefore recommended as not eligible for listing in the National Register.



FIGURE 104. 3637 15TH AVENUE SW, HOUSE, LOOKING SW

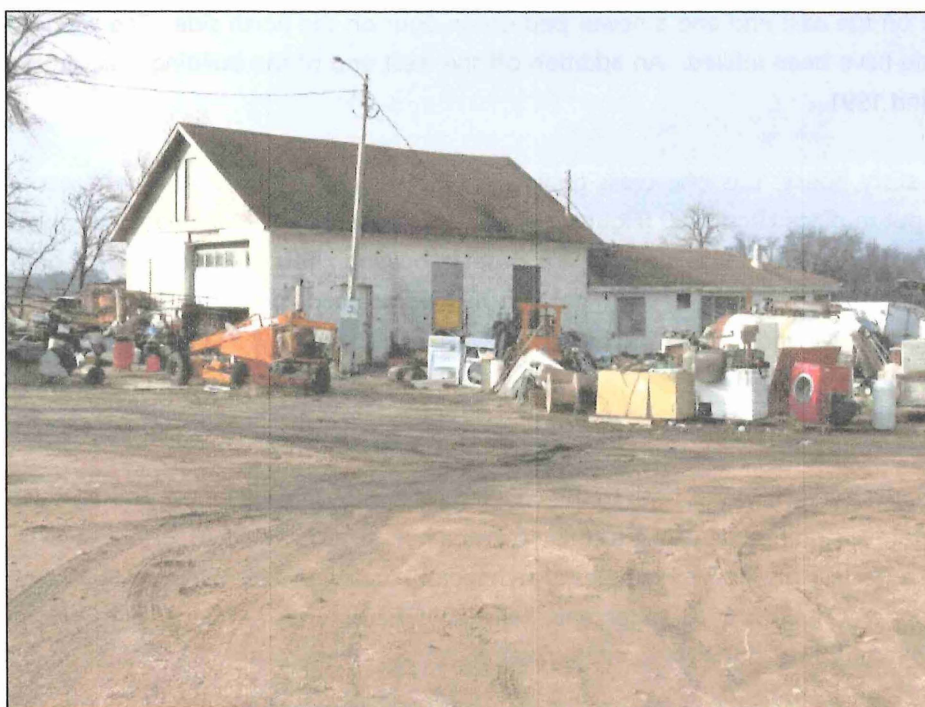


FIGURE 105. 3637 15TH AVENUE SW, MAINTENANCE BUILDING, LOOKING SW



FIGURE 106. 3631 15TH AVENUE SW, HOUSE, LOOKING NW

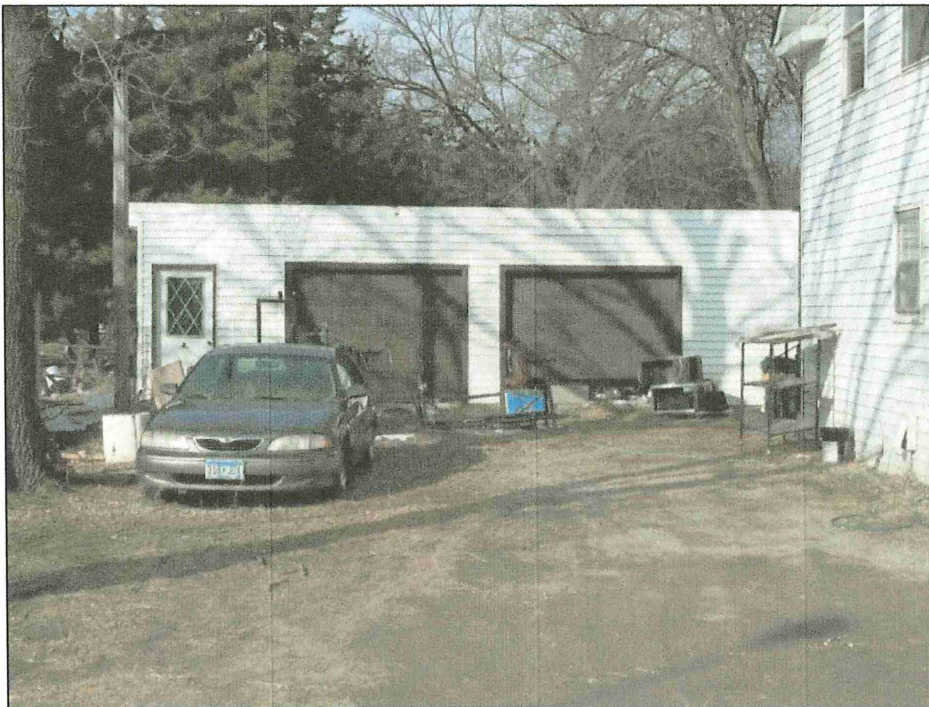


FIGURE 107. 3631 15TH AVENUE SW, GARAGE, LOOKING W

4.20 KH-WLC-135

House

3055 Highway 12 W

Based on a conversation with its current owner, this house was constructed for his parents in 1965. It is a one-story, L-plan house with an attached three-bay garage, and it has a concrete-block foundation (Figure 108). The property owner indicated that upon his parents' passing, the third, easternmost bay of the garage was added on, the entirety of the house and garage was clad in vinyl siding, and the vinyl soffits were added. The house retains several original 1/1 double-hung sash windows and Chicago-style picture windows, the latter located on the back (north) side of the house. Windows on the front (north) of the east ell of the house appear to be added or replacement sliding windows. Insets containing a row of five glass blocks are located in the basement level on the north end of the house, while fixed, single-light windows are in the same level on the west side. The cross-gabled roof is clad in asphalt shingles, and an interior brick chimney extends above the peak of the ell.

4.20.1 Recommendations

This house has no important associations with historically significant events, patterns, or persons, and it therefore does not meet National Register Criterion A or B. It is a modest, fairly non-descript, and modified house that does not exemplify a specific architectural style; therefore it does not meet Criterion C. This property is therefore recommended as not eligible for listing in the National Register.



FIGURE 108. 3055 HIGHWAY 12 W, HOUSE, LOOKING SW

4.21 KH-WLC-136

House

101 33rd Street NW

This one-and-a-half-story, side-gabled house with concrete-block foundation was built in 1957 (Figure 109). The house has been altered with the addition of a shed-wing, open front porch; vinyl siding, soffits, and fascia; and replacement doors and windows. The middle section of the house, which connects to the garage, extends in an incongruous fashion off the rear of the house, suggesting it was expanded or added between the original house and garage prior to the re-siding. The roof is covered in asphalt shingles, and an interior brick chimney rises above the peak. A modern, detached garage/workshop is located to the southeast of the house (Figure 110).

4.21.1 Recommendations

This house has no important associations with historically significant events, patterns, or persons, and it therefore does not meet National Register Criterion A or B. It is a modest, fairly non-descript, and modified house that does not exemplify a specific architectural style; therefore it does not meet Criterion C. This property is therefore recommended as not eligible for listing in the National Register.



FIGURE 109. 101 33RD STREET NW, HOUSE, LOOKING SW



FIGURE 110. 101 33RD STREET NW, GARAGE, LOOKING SW

4.22 KH-WLC-137

House

103 33RD Street NW

This circa-1965 house with concrete-block foundation appears to have been converted to a two-story house through a dormer addition on the south side of a gable-roofed house (Figure 111). In all likelihood, it was a side-gabled house that was, like its two neighbors to the north, not oriented toward the road. It has been additionally altered through the installation of vinyl siding, soffits, and fascia, replacement windows and doors, and decks on the east and west ends. A new, side-gabled entrance has been constructed below the dormer addition. Just to its south is a detached garage (Figure 112).

4.22.1 Recommendations

This house has no important associations with historically significant events, patterns, or persons, and it therefore does not meet National Register Criterion A or B. It is a modest, fairly non-descript, and modified house that does not exemplify a specific architectural style; therefore it does not meet Criterion C. This property is therefore recommended as not eligible for listing in the National Register.



FIGURE 111. 103 33RD STREET NW, HOUSE, LOOKING NW



FIGURE 112. 103 33RD STREET NW, GARAGE, LOOKING W

4.23 KH-WLC-138

House

105 33rd Street NW

This circa-1960, one-and-a-half-story, side-gabled house has a concrete-block foundation. It is not oriented toward the road (Figure 113). It has been clad in vinyl replacement siding, and also has vinyl soffits and fascia. A deck has been added at the front of the house. Fenestration includes a Chicago-style picture window, 1/1 double-hung sash windows, and a newer basement egress window on the west end of the house. Opposite the egress window on the east end of the house is an inset containing a row of four glass blocks. The roof is clad in asphalt shingles. A large, modern garage is located east of the house (Figure 114).

4.23.1 Recommendations

This house has no important associations with historically significant events, patterns, or persons, and it therefore does not meet National Register Criterion A or B. It is a modest, fairly non-descript, and modified house that does not exemplify a specific architectural style; therefore it does not meet Criterion C. This property is therefore recommended as not eligible for listing in the National Register.



FIGURE 113. 105 33RD STREET NW, HOUSE, LOOKING NW



FIGURE 114. 105 33RD STREET NW, GARAGE, LOOKING NE

4.24 KH-WLC-139

House

107 33RD Street NW

This circa-1930, front-gabled bungalow with concrete-block foundation is all that remains of a farmstead formerly at this location (Figure 115). It is not oriented toward the road. The house has been altered by the enclosure of the front porch, the addition of shed dormers on both sides of the roof, a shed-roof addition to the back of the house, and the installation of vinyl siding and soffits, replacement windows and doors, and a deck off the west side. A later, vinyl-sided, two-car garage is located north of the house (Figure 116).

4.24.1 Recommendations

This house has no important associations with historically significant events, patterns, or persons, and it therefore does not meet National Register Criterion A or B. It is a modest, fairly non-descript, and modified house that does not exemplify a specific architectural style; therefore it does not meet Criterion C. This property is therefore recommended as not eligible for listing in the National Register.



FIGURE 115. 107 33RD STREET NW, HOUSE, LOOKING SW



FIGURE 116. 105 33RD STREET NW, GARAGE, LOOKING NW

4.25 KH-DTC-001

County Ditch No. 46

The petition for County Ditch No. 46 was submitted by Andrew Larson and others on February 13, 1919, and an order established by the Kandiyohi County Board on March 13, 1919, for an engineer to conduct a preliminary survey of potentially affected lands (*The Willmar Tribune* 1919a). Following the completion of the remaining procedures required by Minnesota law (see Section 5.1, below), an order establishing the ditch was made on August 29, 1919 (*The Willmar Tribune* 1919b). Bids for construction were received on July 15, 1920, and construction of the ditch was completed circa 1922 (*The Willmar Tribune* 1920, 1921).

County Ditch No. 46 is an open ditch constructed to be eight feet deep with a one-to-one slope ("General Specifications for the Construction of County Ditch Number 46. Kandiyohi County, Minnesota," Minnesota Division of Waters, County and Judicial Ditch Files, held at the Minnesota Historical Society, St. Paul) (Figure 117). It begins approximately ½ mile east of 30th Street SW, adjacent to the north side of 30th Avenue SW, on the southern edge of Section 21 of T119N, R35W, then extends west for approximately 2-1/4 miles, where it meets County Ditch No. 10 in Section 19 of T119N, R35W, placing approximately one mile of the ditch within the APE. Concrete box culverts carry 19th Avenue SW and CR 55 over the ditch within the APE (Figures 118 to 119).

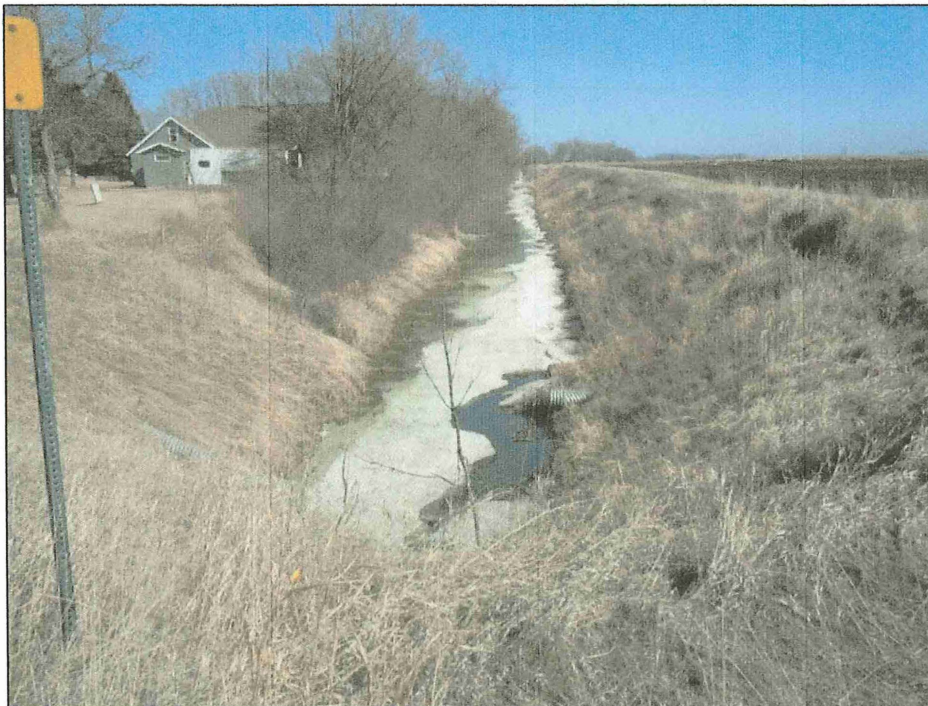


FIGURE 117. COUNTY DITCH NO. 46 AT 19TH AVENUE SW, LOOKING SE



FIGURE 118. COUNTY DITCH NO. 46 AT 19TH AVENUE SW, LOOKING NW



FIGURE 119. COUNTY DITCH NO. 46 AT COUNTY ROAD 55, LOOKING SE

4.25.1 Recommendations

County Ditch No. 46, while associated with the pattern of artificial drainage that expanded farming and opened up previously unviable areas for Euroamerican settlement in Minnesota, is not associated with this pattern in an important way. It was not one of the earliest, did not have an important impact on the development of farming in Kandiyohi County or the Willmar area, and did not open up any new areas of the state for settlement. It therefore does not meet Criterion A. It is not strongly associated with any particular individual and therefore could not meet Criterion B. As a standard, open ditch, it does not incorporate outstanding design or engineering or exhibit any distinctive characteristics, and it therefore does not meet Criterion C. County Ditch No. 46 is therefore recommended as not eligible for listing in the National Register.

4.26 KH-DTC-002

County Ditch No. 10

This property was determined to be potentially eligible for listing in the National Register by MnDOT prior to the Phase I survey, and it is addressed in the following chapter.

4.27 XX-RRD-038

Willmar and Sioux Falls Railway Main Line

The Willmar and Sioux Falls Railway main line was built between 1887 and late 1888 and has not previously been evaluated. It is recommended as potentially eligible for listing in the National Register because it is a railroad corridor built between 1862 and 1956, a property type defined in and subject to the registration requirements provided by the Railroads MPDF. It is further addressed in the chapter following the one on County Ditch No. 10.

PHASE II RESULTS - KANDIYOHI COUNTY DITCH NO. 10 (KH-DTC-002)

Kandiyohi County Ditch No. 10 was evaluated for its National Register eligibility. Based on the evaluation, it is recommended as not eligible for listing in the National Register. The property was evaluated with reference to the historic contexts provided below.

5.1 THE DEVELOPMENT OF ARTIFICIAL DRAINAGE IN MINNESOTA, 1860-1920

When, in his 1861 address to the legislature, Governor Alexander Ramsey (1861:20) estimated 5,000,000 acres of "swamp lands" that could be easily reclaimed in Minnesota, these state-owned acres constituted less than half of the total "unfit for cultivation" (Palmer 1915:88) at that time due to water content, occupying approximately 20 percent of the land area in the state (Marsden 1922:539). In 1860, as had occurred with other states admitted to the union, Minnesota was legally conveyed the acreage to which Ramsey referred by the federal government under an extension of the Swamp Land Act, with the express condition that proceeds garnered from their sale or direct appropriation in kind would be applied to constructing levees and drainages to reclaim them, "as far as necessary" (Palmer 1915:18, 88).

The necessity of artificial drainage was, in the main, the creation of viable agricultural land or the improvement of existing agricultural land. A 1916 bulletin published by the United States Department of Agriculture summarized the improvements that could be achieved, which were likely to result in increased yields and, by extension, profits (Smith 1916:23-24). These included the formation of more granulated, porous, and friable soil, which improved growing conditions and decreased erosion and flooding; warming of the soil, which extended the growing season, lessened the potential for frost damage in the spring and fall, and promoted germination; and the facilitation of cultivation: "Land can be plowed earlier in the spring and is better pulverized. Crops can be cultivated sooner after a rain, and by closing small ditches with tile drains more improved machinery can be used and the cost of cultivation decreased."

Although creating and improving agricultural land was the expressed intention for constructing artificial drainages, couched within it was the need to accommodate the growing numbers of arrivals to Minnesota from the east and overseas by supplying some with the means of a livelihood and supplying others with sustenance via that livelihood. Further, when drainage allowed for more prospective farmers to settle in the state, particularly the western part, it had the effects of proliferating markets, creating railroad traffic, and increasing the tax base, promoting the state's economic well-being (Prince 1997:242). Artificial drainages additionally provided health benefits by decreasing mosquito habitats, resulting in significant reduction in incidences of malaria (Palmer 1915:2-3; McCorvie and Lant 1993:25-26). From a practical perspective, public drainage construction facilitated road building, providing land for and subsequently preventing flooding of roads; further, the spoils from ditch construction were used for road-building material (Beauchamp 1987:17; State Drainage Commission 1909:27).

Prior to 1860, the only legislative action pertaining to artificial drainage in Minnesota was the 1858 passage of a state law allowing for the formation of corporations to drain presumably non-federally owned lands

and establishing the conditions under which they would be permitted to construct ditches (Palmer 1915:59). As people began to settle into workaday life following the Civil War, however, many of whom turned to agriculture, and as railroads began to extend to the western, wetter part of the state, encouraging new settlement there, a series of laws were enacted to address artificial drainage. These became increasingly broader over time with regard to jurisdictional responsibility and spatial extents involved, so that the first law, enacted in 1866, allowed for a justice of the peace to hear cases in which a property owner was refused permission by another property owner to ditch their lands as he found necessary to drain his own; laws enacted in the 1870s shifted this responsibility from the justice of the peace to town supervisors (1877), allowed the latter to open ditches to drain public highways (1874), and established the procedures they should follow when proposed ditches or drains were to cross town boundaries (1879); and a law enacted in 1883 removed the responsibility of oversight of proposed ditches or drains over town boundaries from the town supervisors to the county commissioners (Palmer 1915:62). In all cases, the burden of payment for the construction of the ditches was borne by those private landowners whose lands were determined to benefit from said construction, through subsequently levied taxes or assessments.

The 1883 legislation largely set the procedures by which drainage construction was to be proposed and approved for at least the next four decades. Interested landowners were required to create, sign, and file with the county auditor a petition for construction of a ditch, which would describe its proposed general route. Three uninterested viewers would then be appointed to survey a specific route with a civil engineer, estimate construction costs, identify landowners who were to benefit from the ditch and determine the division of responsibility among them for the cost of construction, develop a general plan for its construction, assess whether the ditch would be of public good, assess property damage that might result from construction, and file a report with the county auditor. If the viewers reported that the ditch would be of public good, after a period of public notification and, if applicable, addressing any filed protests, the commission would hold a public hearing to make an official determination that the ditch would be “of public utility or conducive to public health, or of public benefit or convenience” (General Laws of Minnesota for 1883, Chapter 108, Section 10) and order construction, subject to a 30-day period for appeals (Palmer 1915:62).

At the time this law was passed, Minnesota had long since granted roughly two million acres of its swamp lands to railroads, and legislated that the proceeds of whatever swamp lands that were or would be owned by the state would be used to help finance state institutions and the common school fund, with no regard for the federal directive to use those proceeds for reclamation (Palmer 1915:94). The resultant absence of revenue for reclamation efforts is a major reason that “up to 1893 no public drainage work of any magnitude had been done in the state” (State Drainage Commission 1909:26). A lack of any other designated funding, organized effort for ditch construction, or dedicated authority to direct and oversee such an effort kept drainage construction at a minimum through the end of the nineteenth century (Palmer 1915:95).

The first notable attempt at remedying these problems came in 1886 through conventions in the Red River Valley instigated by the St. Paul Minneapolis and Manitoba Railway Company and attended by representatives and residents of Marshall, Polk, Norman, Clay, and Wilkin counties. These resulted in a

valley-wide topographic survey to determine drainage needs and the locations that would meet them; the drafting of a comprehensive general drainage law, which among other elements allowed for the "organization of drainage districts as municipal corporations" (*Crookston Times* 1886) and the creation of a "state drainage commission . . . to approve and supervise . . . the work of the districts"; and a resolution calling on the governor to advise the legislature of the need for state funding to construct drainages in the Red River Valley. Although such funding did not gain approval in 1887, important revisions were made to general drainage laws that year, including allowance for any county to be organized as a drainage district with a permanent board of drainage commissioners having oversight of all drainage-related activities (Palmer 1915:68).

More progress was made in 1893, when the legislature made its first appropriation for drainage construction, \$100,000 divided equally over four years to be used in constructing drainages in the Red River Valley, and established a precursor to the State Drainage Commission to oversee the work. This precursor, the Red River Board of Audit, included the governor, the secretary of state, the Great Northern's chief engineer, and a representative of the counties. The Board of Audit continued to supervise construction and financial matters for another seven years, but the supervision of annual inspections, repairs, and the like was taken over in 1897 by an entity called the Board of State Drainage Commissioners, although at that time it was concerned only with the Red River Valley (Palmer 1915:69).

The first decade of the twentieth century marks the beginning of the first systematic activity in public drainage, bolstered by a multitude of legislative actions that effected the much-needed authority, effort, and funding necessary to developing a full-scale drainage system. In 1901, for example, the Minnesota State Drainage Commission was established to identify, locate, and construct drainages in state-owned wetlands throughout the state, with an initial appropriation of \$25,000 a year in 1901 and 1902 (General Laws of Minnesota for 1901, Chapter 90). The year 1905 witnessed the consolidation and revision of all existing laws pertaining to the procedures for constructing county and judicial ditches to create a comprehensive drainage law for the state (Palmer 1915:70-71). Between 1907 and 1911, over \$500,000 was allocated to the State Drainage Commission, and another \$686,350 was obtained by them through assessments against private lands, allowing for drainage construction, various surveys, and improvements to existing drainages (Palmer 1915:96-97). The Volstead Act of 1908 provided for government-owned lands to be assessed in the same way as those privately owned, providing additional funds for reclamation and creating less resistance to drainage construction by private property owners who, as a result of the act, would be less financially burdened by it. In 1909, the State Drainage Commission reported that the work done throughout the state in 1907 and 1908 in creating drainage systems equaled all of the work completed in the 50 years prior (State Drainage Commission 1909:26-27).

By 1915, it was reported that over seven million of Minnesota's original ten million acres of swamp lands had been reclaimed by county, judicial, and state ditches (Palmer 1915:98). Between 1915 and 1920, another approximately two million acres were added to the total by the same (Marsden 1922:543). Of the 9,232,709 acres in Minnesota for which public ditches had been completed or were under construction, according to the U.S. Census of 1920, 66,279 were located in Kandiyohi County (Marsden 1922:551).

5.2 COUNTY DITCH CONSTRUCTION IN KANDIYOHI COUNTY

In September of 1898, *The Willmar Tribune* reported "The people of the county seem now to be thoroughly aroused to the importance of draining the rich lands which heretofore have been useless on account of surplus water. Never before have so many petitions for public drains been filed with the county auditor. The county board is loaded down with ditch business . . ." (*The Willmar Tribune* 1898a).

The "ditch business," as it were, of Kandiyohi County began with the July 29, 1885, order to establish a ditch in Willmar and Kandiyohi townships, which would be designated County Ditch No. 1 (Lawson 1905:387). After the establishment of two additional ditches in 1886, County Ditch No. 2 in Willmar and County Ditch No. 3, which augmented County Ditch No. 1, public drainage activity was slow and sporadic, with County Ditch Nos. 4 through 7 established, respectively, in the townships of Whitefield and Falun, Irving, Whitefield, and St. Johns between 1888 and 1895. Following, the most intensive period of county drainage activity led to the establishment of an additional 43 county ditches between 1898 and 1921. The County and Judicial Ditch files of the Minnesota Division of Waters do not contain a file for County Ditch No. 51. The remaining 14 Kandiyohi County ditches were constructed between the late 1940s and 1986 (Minnesota Division of Waters, County and Judicial Ditch Files, held at the Minnesota Historical Society, St. Paul; Kandiyohi County 2016).

5.3 KANDIYOHI COUNTY DITCH NO. 10

The petition for County Ditch No. 10 was filed by L. O. Thorpe and others in April of 1898 (Lawson 1905:388; Chamberlain 1977). It was presented at a meeting of the board of county commissioners on May 16, 1898, where it was determined that the ditch would be of public benefit and the petition was accepted. The appointed viewers were H. B. Eddy of Whitefield Township, S. A. Syverson of Arctander Township, and John G. Flann of Lake Lillian Township (*The Willmar Tribune* 1898b). The viewers' report was accepted by the board and the ditch designated as Public Ditch No. 10 by the same in September of 1898 (*The Willmar Tribune* 1898c). The sale of ditching jobs for County Ditch No. 10 was held on November 12, 1898, and it was constructed between 1898 and 1901 (*The Willmar Tribune* 1898d; Chamberlain 1977). Once completed, County Ditch No. 10 consisted of 9.9 linear miles of open ditch beginning at Foot Lake in the northwest quarter of Section 16, Willmar Township and ending at Hawk Creek in Section 10 of Edwards Township (Lawson 1905:388).

In 1915, a petition was filed to repair the ditch by making 9.8 miles of the ditch wider and deeper, which was approved in 1916 and carried out in 1917 and 1918 (Chamberlain 1977). In 1974, a petition was made to improve the ditch through re-sloping several sections and removing a number of trees, citing that "the ditch as now constructed does not afford proper adequate drainage of the lands adjacent thereto" (Petition to Kandiyohi County Board of Commissioners, May 1, 1974, contained in Minnesota Division of Waters, County and Judicial Ditch Files, held at the Minnesota Historical Society, St. Paul). These improvements were carried out sometime between 1979 and 1983 ("Plans, Improvement C, County Ditch No. 10, Kandiyohi County, Minnesota," available online at http://www.co.kandiyohi.mn.us/departments/drainage/ditch_construction_plans_and_maps.php).

5.3.1 Description

County Ditch No. 10 is an open ditch. Based on the most recent improvement plans, it ranges from 8 to 12 feet deep at the bottom and has a two-to-one slope ("Plans, Improvement C, County Ditch No. 10, Kandiyohi County, Minnesota," available online at http://www.co.kandiyohi.mn.us/departments/drainage/ditch_construction_plans_and_maps.php) (Figure 120). It begins at Foot Lake, then runs west and south, ending at Judicial Ditch 7 in Edwards Township. Comparison of aerial photographs indicates that the ditch follows its originally constructed route, with the exception of a small alteration in Section 17, T119N, R35W, where it was rerouted to accommodate a runway for the first Willmar Airport. Within the APE, concrete box culverts carry TH 12, CSAH 5 (30th Street NW), and CR 55 over the ditch, and the ditch runs under CR 40 (15th Avenue SW) via a corrugated metal pipe culvert (Figures 121 to 123).

5.3.2 Significance

County Ditch No. 10 was established in 1898, 13 years after the first county ditch was established in Kandiyohi County; therefore it was not among the earliest ditches constructed. By that time, west-central Minnesota had long been open to Euroamerican settlement and farming, so the ditch did not open any new areas of the state for such. County Ditch No. 10 was one of many constructed in succession to improve farmlands, and its establishment came easily through the standard legal process without any marked public involvement or political complications that might impact Minnesota drainage law. While it certainly would

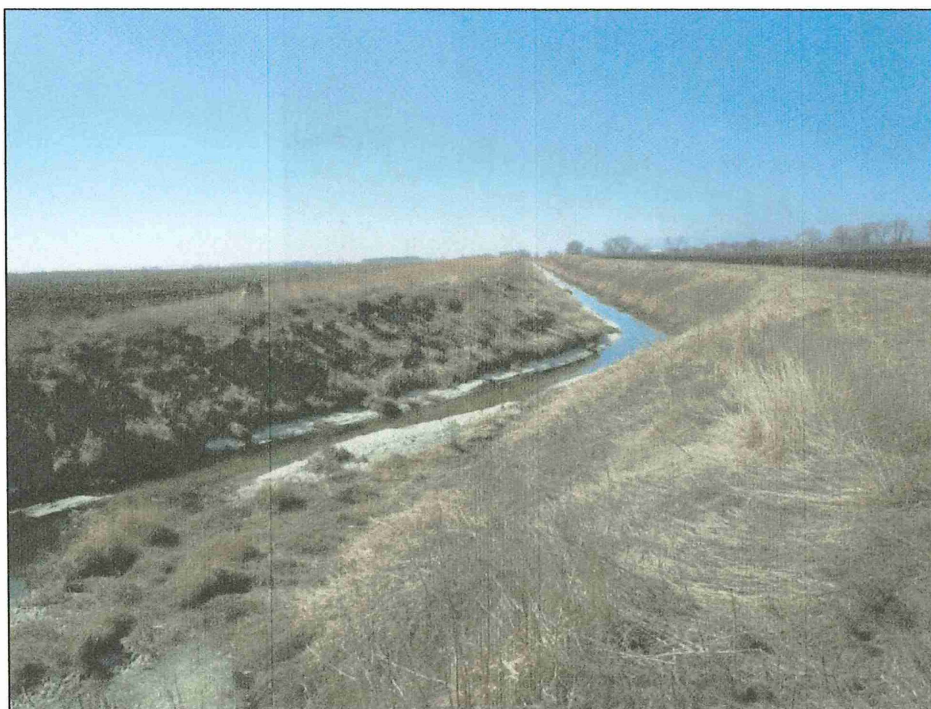


FIGURE 120. COUNTY DITCH NO. 10 AT COUNTY ROAD 55, LOOKING W



FIGURE 121. COUNTY DITCH NO. 10 AT 30TH STREET NW, LOOKING E-SE



FIGURE 122. COUNTY DITCH NO. 10 AT COUNTY ROAD 55, LOOKING E-SE



FIGURE 123. COUNTY DITCH NO. 10 AT COUNTY ROAD 40, LOOKING S-SW

have improved surrounding farmland as had intended, no evidence suggests that its impact had a significant effect on the agricultural economy of Willmar. For these reasons, County Ditch No. 10 does not meet Criterion A. The individuals involved in petitioning for County Ditch No. 10 were not significant with regard to agriculture or public drainage history; therefore, the ditch does not meet Criterion B. The ditch, as a basic, open ditch does not employ innovative design, construction, or engineering, and it therefore does not meet Criterion C.

5.3.3 Recommendations

Because it does not meet National Register significance criteria, County Ditch No. 10 is recommended as not eligible for listing in the National Register.

PHASE II RESULTS – WILLMAR AND SIOUX FALLS RAILWAY COMPANY MAIN LINE (XX-RRD-038)

The Willmar and Sioux Falls Railway Company main line was evaluated for its National Register eligibility. Based on the evaluation, it is recommended as not eligible for listing in the National Register. The property was evaluated with reference to the historic contexts and registration requirements provided in the Railroads MPDF.

6.1 THE WILLMAR AND SIOUX FALLS RAILWAY

The history of the Willmar and Sioux Falls Railway is provided in the Railroads MPDF (Schmidt et al. 2007:E-97) as follows:

In 1886, the Willmar and Sioux Falls Railway Company was formed by James J. Hill "to build and operate a railroad and telegraph line from Willmar through Granite Falls, Marshall, and Pipestone to Sioux Falls by a feasible route" (Prosser 1966:169). The formation of the company was prompted by the movements of the Minneapolis and Pacific [Railway Company], which sought to tap into the [St. Paul Minneapolis and] Manitoba's wheat market west of Minneapolis, "thereby securing ample raw materials and reasonable rates for the growing flour milling industry at Minneapolis" (Luecke 1997:116).

When it became clear that the Minneapolis and Pacific would become more than a paper road, a line was built in 1885 from St. Cloud to Willmar by the Manitoba-backed St. Cloud Mankato and Austin [Railroad Company] (see above). Subsequently, between 1887 and 1888, the recently formed Willmar and Sioux Falls built a line from Willmar to Sioux Falls, running in Minnesota from Willmar to Jasper and then extending southwest to Sherman, South Dakota, on its way to Sioux Falls. The line, completed in late 1888, provided a connection between Sioux Falls and Lake Superior via the Willmar to St. Cloud line of the former St. Cloud Mankato and Austin, the Manitoba line from St. Cloud to Hinckley, and the soon-to-be-completed Eastern [Railway Company] line from Hinckley to Superior. The Willmar and Sioux Falls, along with the rest of these connections, was subsumed by the Great Northern in 1907 (Hidy et al. 1988:63; Luecke 1997:115-131).

6.1.1 Description

Within the project APE, the railroad line consists of a single track incorporating wood ties and steel rails supported by a slightly raised rail bed (Figure 124). The railroad line remains active as part of the BNSF system.

6.1.2 Significance

The Willmar and Sioux Falls Railway line was built beginning in 1887, well after the southern half of western Minnesota had been opened to Euroamerican settlement by the previous construction of other railroad



FIGURE 124. WILLMAR AND SIOUX FALLS RAILWAY AT COUNTY ROAD 55, LOOKING SW

lines, including the St. Paul Minneapolis and Manitoba main line, which ran through Willmar. Although the Willmar and Sioux Falls ultimately allowed for transportation between Lake Superior and both western Minnesota wheat and Sioux Falls, South Dakota, it did so via the previously built East St. Cloud to Hinckley (1882) and East St. Cloud to Willmar (1885-1886) lines and the concurrently/subsequently built Eastern Railway Company line from Hinckley to Lake Superior (1888-1889), thus it did not on its own connect wheat or Sioux Falls to Lake Superior. The Willmar and Sioux Falls Railway line was not influential within the state's railroad network in any regard, and as a line built from Willmar west beginning in 1887, it did not make important early connections within the network. It additionally did not provide a critical link or junction between important corridors, because at the time that the Willmar and Sioux Falls line was built, Sioux Falls was the western terminus, i.e., no Manitoba-associated corridors with which to link were present at the west end (Hufstetler and Bedeau 2007:97). For these reasons, the Willmar and Sioux Falls Railway main line does not meet registration requirements for significance under Criterion A.

The registration requirements dictate that railroad corridors will not be eligible under Criterion B or C, and are unlikely to be eligible under Criterion D. With regard to the latter criterion, no evidence is present to suggest that "further analysis of the [Willmar and Sioux Falls Railway] corridor [would] be likely to yield important information about significant aspects of the evolution or development of railroad corridor design, operations, or the inter-relationships between railroads and the industrial and commercial operations they served" (Schmidt et al. 2007:197), as would be necessary for the corridor to be eligible.

6.1.3 Recommendations

Because the Willmar and Sioux Falls Railway main line does not meet the registration requirements for significance established by the Railroads MPDF, it is recommended as not eligible for listing in the National Register.

SUMMARY OF RECOMMENDATIONS

In addition to identifying one property that previously was determined eligible for listing in the National Register, the Phase I architectural history survey for the Willmar Wye Project identified 26 architectural history properties within the APE. Of these, 24 properties were recommended as not eligible for listing in the National Register. The remaining two properties, County Ditch No. 10 (KH-DTC-002) and the Willmar and Sioux Falls Railway main line (XX-RRD-038) were subject to a Phase II evaluation. Both were found to be not eligible due to a lack of significance.

7.1 PROJECT RECOMMENDATIONS

No further architectural history work is recommended in relation to the surveyed properties prior to or during construction for the Willmar Wye project. The St. Paul and Pacific Railroad Mainline: St. Anthony to Breckenridge RR Corridor Historic District is eligible for listing in the National Register; therefore any further work is pending MnDOT's determination of the effects of the project, if any, on this property.

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